

# Desert Shores Motor Coach Resort

Amendment No. 1  
Specific Plan No. 99-10-10

Prepared For:

**City of Indio, California**

Prepared By:



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## 1.0 Introduction

The Desert Shores Motor Coach Resort currently consists of an exclusive motor coach resort of no more than 141 motor coach spaces. The project lies in the City of Indio on about 60 acres west of Indian Palms Country Club. Hjorth Street to the west, 49<sup>th</sup> Street to the south and 48<sup>th</sup> Avenue to the north bound the project site (see Figure 1, Vicinity Map).

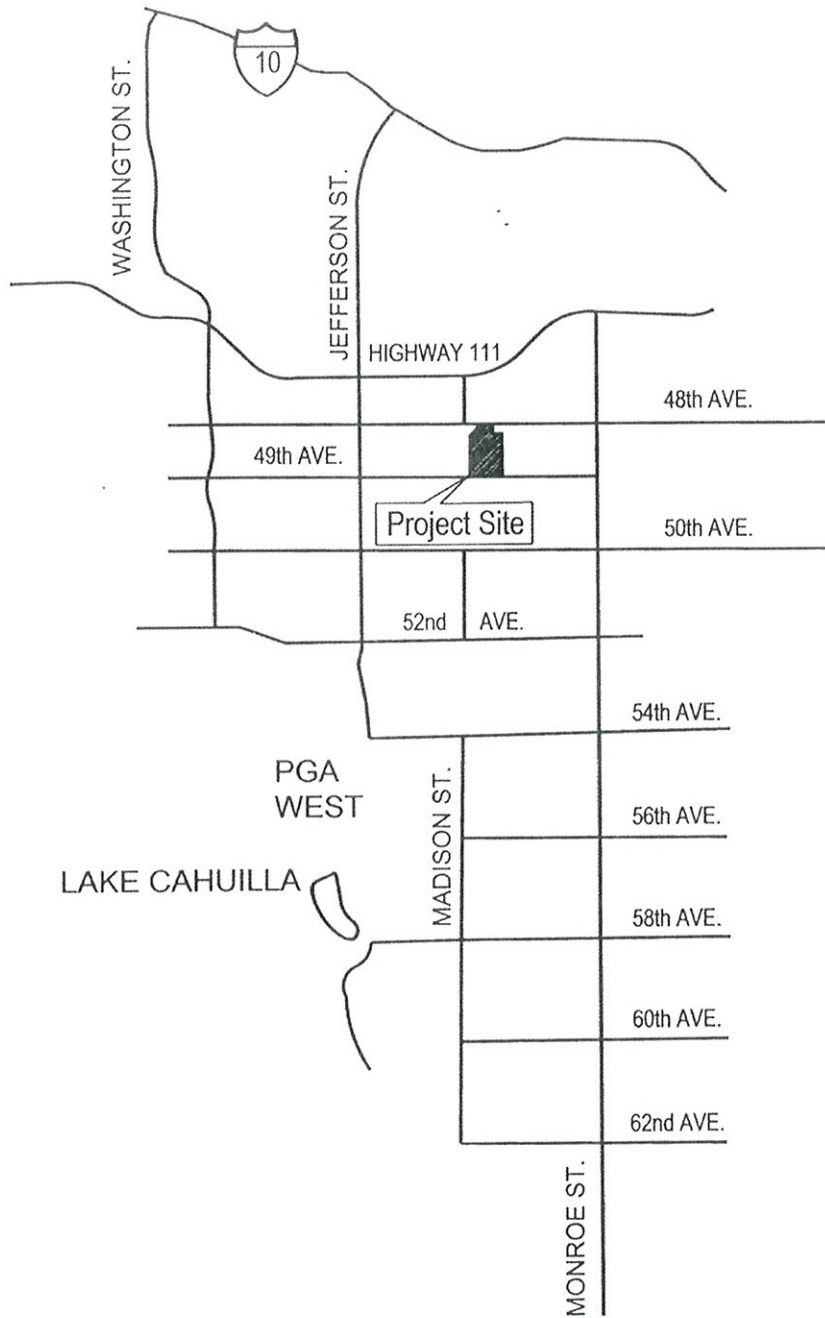
In 1999, the City of Indio approved a Specific Plan that outlined the approved uses and design parameters to allow the development of the motor coach resort. More specifically, the project provides an exclusive motor coach resort environment including a clubhouse with recreational amenities such as swimming pools, tennis courts, putting course, etc., on-site parking, on-site storage, four lakes and landscaping corridors surrounded by a maximum of 141 motor coach spaces. The original Specific Plan served as a guide for the character, design and standards for development of the motor coach resort. It also included a degree of flexibility to allow a response to future market changes.

The original Specific Plan also included a 13-lot subdivision on the northern portion of the site, adjacent to and immediately west of, the Santa Rosa tract. However, during the review and approval process, the parties agreed to eliminate the residential subdivision portion of the project and dedicate the property to the City for park or open space purposes.

The original Specific Plan implemented a design scheme for the project. Of particular note to this amendment, was the specific motor coach spaces themselves. The existing Specific Plan allows for the construction of a garage and patio in conjunction with the motor coach space. The intent was to provide for a place to put a car, often times towed behind an motor coach, and storage space for personal belongings. A patio cover with cooking facilities was also allowed but those cooking facilities could not be enclosed by permanent walls. Roll up doors were allowed to secure the kitchen equipment.

A Tentative Tract Map (TTM) accompanied the Specific Plan request. The TTM showed the proposed 141 lots envisioned in the Specific Plan and formally created in the subdivision map. The maps have been filed so all the 141 lots have been created. This permits the developer to sell the lots and the owners to somewhat personalize them to their specific needs. The overall requirements of the Specific Plan must be followed however.

Finally, the original application included a General Plan change from Residential Low Density (RL) to a Medium Density Residential – Mobile Home/RV Park (RM-MH) designation and a zoning classification amendment from R-1-7 (Single Family Residential – 7,000 square foot minimum lot size) to Medium Density Residential with a Mobile Home/RV Park (RM-MH) zoning classification. RV parks are an allowed use subject to the Specific Plan implementation requirements. Those changes were approved along with the Specific Plan approval.



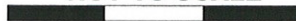
**VICINITY MAP**

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**NOT TO SCALE**



**Figure 1**

The clubhouse, the storage areas, some of the amenities and about one-half of the motor coach spaces have been built to date. That means that approximately 65-75 motor coach spaces remain to be constructed. Since the time of approval of the Specific Plan, the motor coach space owners have identified specific needs as a result of their actual use of the motor coach space so the needs of the owners have changed from the original intent. Today, owners within the resort want the option to permanently enclose the kitchen areas and build an enclosure for their motor coach. The motor coach owners identified the need for protection of their units from the elements during extended stays. The architectural enclosure is also seen as an aesthetic enhancement. Permanent enclosure of the kitchen areas allows for a more substantial improvement to the structure and facilities providing a better living experience while visiting the City.

The purpose of this amendment is to change the Specific Plan to allow options for the space owners for that change in the marketplace since the original concept for the resort was approved. Options will include:

1. Development on the lot as originally envisioned in the Specific Plan which included a Motor coach space, a garage and a patio with kitchen facilities.
2. Development on the lot as originally envisioned in the Specific Plan but enclosure of the kitchen facilities (a casita).
3. Development on the lot as envisioned in this amendment which includes the garage, casita and a motor coach port.

Other permutations are also available that will lie somewhere between the original plan and the plans outlined and allowed in this amendment. All development on sites, whether existing or new, will be subject to the limitations contained in the original plan and in this amendment.

There are no other entitlements associated with this request for amendment. An Environmental Assessment, in conjunction with technical studies, was prepared and certified when the Specific Plan was approved in 1999. Mitigation measures and the mitigation monitoring plan incorporated into the Desert Shores Motor coach Resort Specific Plan reduced all impacts to less than significant. Since the proposed changes are minor and no substantive changes are proposed with this amendment, no further CEQA analysis is needed at this time.

## **2.0 Plans, Programs and Guidelines**

All provisions of the original Specific Plan, including Community Concept, Land Use Plan, Circulation Plan, Open Space and Recreation Plan, and Infrastructure and Utilities Plan remain in effect, except as modified by this amendment. In addition, the Community Design Guidelines for the clubhouse, circulation, landscaping, exterior walls, recreational amenities and open space, will continue as specified in the original plan.

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What will change is the design concept for the structure on the motor coach site. This includes an increase in the size allowance for structures on the motor coach spaces, allowance for permanent enclosure of the kitchen area, allowance for more development within the kitchen area, including living, dining and bath facilities, and allowance for the construction of a motor coach port.

Figure 2 shows the new site plan and floor plan concept for the motor coach space. Actual interior floor plans may be somewhat different than depicted, but foot prints of the buildings will be one of the two options represented. The structure will remain an option. There will be up to four different models for the optional building from which motor coach space owners may choose.

Figure 3 represents the elevation options for the buildings. Owners who opt for the building will be limited to one of the four options.

As can be seen, the change includes a motor coach port, kitchen, living, dining and bath facilities and a one or two car garage and storage space. There will be options for owners as to whether or not they want the building and what additional building they want. For example, an owner could choose to have only the garage and storage and leave off the motor coach port. An owner may also choose to build a garage and kitchen facility as envisioned in the original plan. They may also want to enclose the kitchen area. All options will remain available. Other options will also exist. Figure 2 represents the maximum square footage for the building, about 2,500 square feet.

It is also possible for owners with existing buildings to remodel their building into the allowable uses and add a motor coach port. In that case, the changes will also be subject to the design limitations outlined in this amendment.

Of particular concern is the motor coach port. The original intent of the motor coach resort was to have people live in the motor coach while on the space and the motor coach would be outside. However, since the possibility exists that people could live in the motor coach in the port, it becomes necessary to adequately ventilate the bus building. Therefore, proper ventilation must be provided in the bus ports per any applicable building code.

The approval of the original Specific Plan allowed covering of the patios with trellises. The owners want to keep that option in order to provide for shade and privacy. However, there needs to be guidelines with regard to the materials of the structures and how the trellises should be developed. In general, it is desirable to have the structures made of wood or a material that simulates wood. No aluminum should be allowed. Also, to assure that the trellises remain open and not enclosed into another room, the trellises should not have walls other than support structures and at least 50% of the cover shall be open. Plants, such as ivies, can be planted and trained to cover the trellises to provide for additional shade.

**Community Design Guidelines**

All the standards contained in the original Specific Plan for Community Design Guidelines (pages 24 through 33) will remain the same except as herein expressly amended. All page numbers referred to below correspond to the page number in the 1999 Specific Plan. Changes are highlighted.

*A. Materials (page 25)*

Roofing materials should be limited to concrete-based or clay tile materials in barrel tile or shake type forms.

The predominant exterior building material should be textured or smooth finish stucco. Wood, tile, *rock* and limited wrought iron should be used as accent materials as dictated by the applicable architectural style.

*B. Add Trellises*

Trellises shall be made of wood or a material that simulates wood. No aluminum should be allowed. Also, to assure that the trellises remain open and not be enclosed into another room, trellises shall not have walls other than support structures and at least 50% of the cover shall be open. Plants, such as ivies, can be planted and trained to cover the trellises to provide for additional shade.

*C. Exhibit 13 (page 27)*

Replace the Exhibit with the new site plans.

*D. Exhibit 13a (after page 27)*

Insert elevations and floor plans for new options.

In addition to the requirements contained in the Specific Plan, the resolutions that approved the original Specific Plan and Tentative Tract Map contained conditions, some of which impact the current request. All original conditions of approval should remain except as specifically amended herein. Changes are highlighted below.

*A. Resolution No. \_\_\_\_\_, a resolution approving the specific plan (99-10-10)*

Section 11, conditions of approval, should be amended as follows:

1. Development shall occur substantially as shown on the submitted plans and as shown on the Desert shores Specific Plan, dated October 31, 1999, or as modified in Amendment No. 1, dated January \_\_\_\_, 2004, and modified herein, unless otherwise approved by the Development Services Director. The project has been modified with the deletion of that portion of the project adjacent to the west of the Santa Rosa tract

and additional design and development standards as outlined in Amendment No. 1; and the applicant/owner shall offer to dedicate to the City of Indio that portion of the property previously included for subdivision purposes. The dedication of property shall be free and clear of any and all encumbrances, liens and hazardous materials. Dedication shall be for park or open space purposes. The City is responsible for improvements, if any, to the property.

3. The applicant or successor in interest shall submit CC&R's, in a form acceptable to the City Attorney, for the motor coach resort. The CC&R's shall include provisions for creation of a property owners' association, and provisions for adequate maintenance of all the common area improvements on the site, including private roadways, lakes, perimeter walls, lighting, landscaping and irrigation. A provision shall be included to prohibit the rental of private garages, and a provision to prohibit habitation in and limit the use of private garages and motor coach port to vehicle storage and related storage only for the owner *unless the motor coach port is built per the specifications established in Amendment No. 1 to the Desert Shores Motor coach Resort Specific Plan*. The CC&R's shall be submitted for review by the Development Services Director prior the issuance of any building permits.

14. *Eliminate this requirement since it is the same as 3, above.*

23. Each motor coach lot may be permitted one accessory structure not to exceed 2,500 square feet of enclosed space and a maximum height of 21 feet. The accessory structures may include a one or two car garage, a kitchen, dining and living area, storage, bath and a motor coach port. A patio may be built and a cover put over it but it cannot be enclosed with walls. All structures must comply with City of Indio Building Codes applicable at the time of submittal for a building permit. All buildings shall comply with design standards contained in the Specific Plan.

25. *Eliminate this condition.*

B. Resolution No. \_\_\_\_\_, resolution approving TTM 29642

Section 3, Conditions of Approval, shall be amended as follows:

1. Development shall occur substantially as shown on the submitted plans and as shown on the Desert shores Specific Plan, dated October 31, 1999, or as modified in Amendment No. 1, dated January \_\_\_\_, 2004, and submitted modified herein, unless otherwise approved by the Development Services Director. The project has been modified with the deletion of that portion of the project adjacent to the west of the Santa Rosa tract and additional design and development standards as outlined in Amendment No. 1; and the applicant/owner shall offer to dedicate to the City of Indio that portion of the property previously included for subdivision purposes. The dedication of property shall be free and clear of any and all encumbrances, liens and hazardous materials. Dedication shall be for park or open space purposes. The City is responsible for improvements, if any, to the property.

2. The applicant or successor in interest shall submit CC&R's, in a form acceptable to the City Attorney, for the motor coach resort. The CC&R's shall include provisions for creation of a property owners' association, and provisions for adequate maintenance of all the common area improvements on the site, including private roadways, lakes, perimeter walls, lighting, landscaping and irrigation. A provision shall be included limiting the use of private garages and motor coach ports to vehicle storage only *unless the motor coach port is built per the specifications established in Amendment No. 1 to the Desert Shores Motor Coach Resort Specific Plan*. The CC&R's shall be submitted for review by the Development Services Director prior to the issuance of any building permit.

*C. Add the following to the Zoning and Development Regulations*

(K) Two off street parking spaces shall be provide per unit. Tandem spaces are acceptable. Guest parking shall be provided at a rate of one space per every four mobile home lots. Any enclosed parking space shall be, at a minimum, 10 feet wide by 20 feet long.

(Q) Trellises are considered structures and, as such, are subject to setback and building code requirements of the City of Indio. Trellises are also subject to design review and approval by the Desert Shores Motor Coach Resort Homeowners Association Design Review Committee. Trellises shall not be enclosed with walls other than support structures. Covers shall be at least 50% open.

(R) Metal patio covers are prohibited.

(S) Pools are subject to setback and building code requirements of the City of Indio. Pools are also subject to design review and approval by the Desert Shores Motor Coach Resort Homeowners Association Design Review Committee.

(T) Bedrooms or other sleeping facilities shall be prohibited within the casita. All sleeping facilities shall be contained the motor coach.

(U) The Motor Coach ports shall be, at a minimum, 65% open on two sides, and have a maximum area of 960 square feet.

(V) Patios shall have a maximum size of 600 square feet.





**DESERT SHORES FLOOR PLAN**

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**Figure 2**



SCHEMATIC  
STREET ELEVATION  
1/8" SCALE

**SCHEMATIC STREET I**

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**DESERT SHORES MOTOR C**  
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**Figure 3**