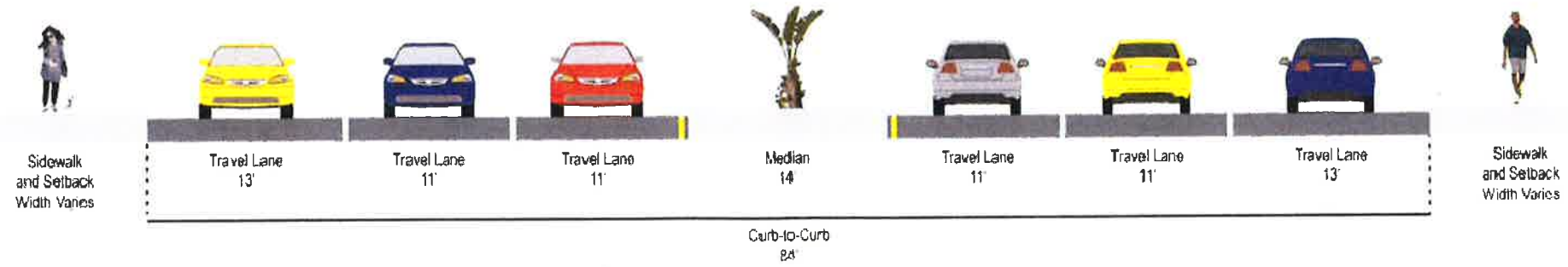
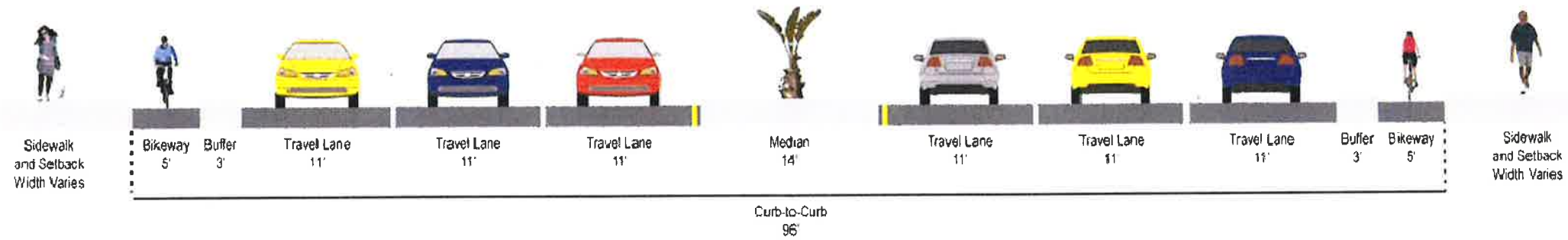


Major Arterial

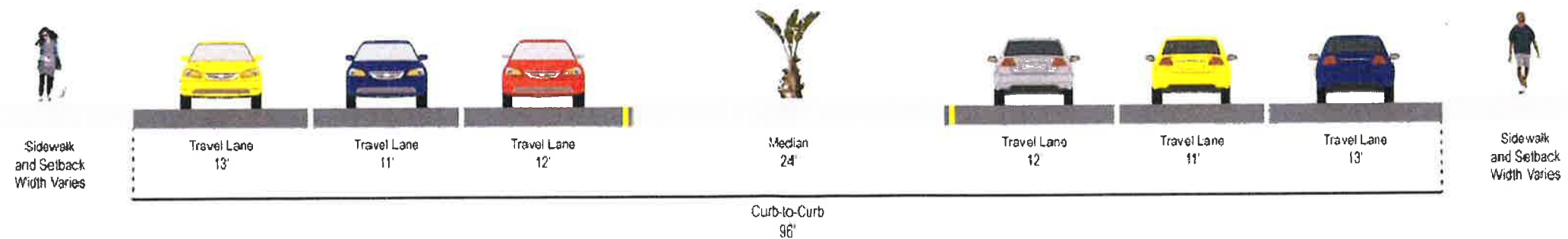
6-Lane Major (84' Curb-to-Curb Width)



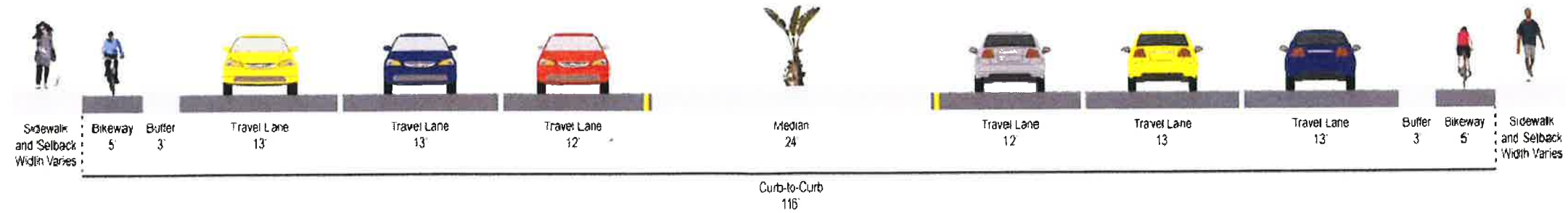
6-Lane Major with Bicycle Facilities (96' Curb-to-Curb Width)



6-Lane Major (96' Curb-to-Curb Width)



6-Lane Major with Bicycle Facilities (116' Curb-to-Curb Width)



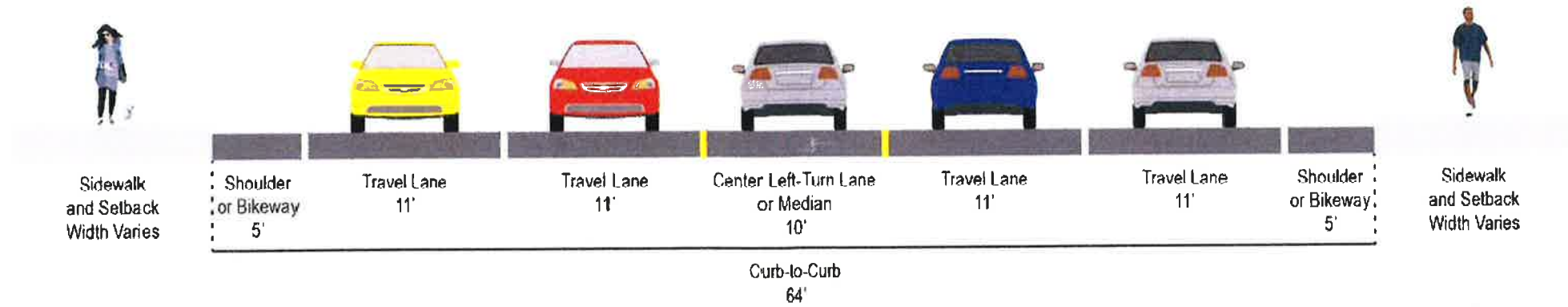
Notes:

- Minimum lane widths are displayed for each curb-to-curb width. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.
- Class II Bike Lanes will have a painted buffer, Class IV Cycle Tracks will have a physical buffer.

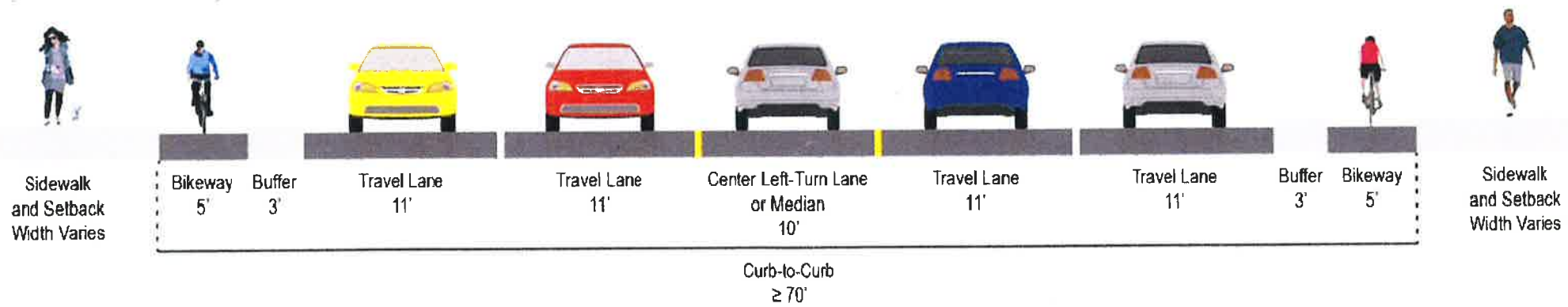
- Washington Street, from Coyote Song Way to northern City limit;
- Jefferson Street, from Avenue 40 to Avenue 50;
- Monroe Street, from Avenue 42 to Fred Waring Drive;
- Jackson Street, from Avenue 42 to north of Kenner Avenue;
- Avenue 42, from Clinton Street to Jackson Street;
- Fred Waring Drive, from Jefferson Street to Monroe Street; and
- Highway 111, from 500' west of Jefferson Street to Rubidoux Street.

Boulevard

4-Lane Boulevard with Median or Center Left-Turn Lane and Optional Bike Lane (64' Curb-to-Curb Width)



4-Lane Boulevard with Median or Center Left-Turn Lane and Bicycle Facilities ($\geq 70'$ Curb-to-Curb Width)*



* Minimum lane widths are displayed for a curb-to-curb width of 70'. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.

Notes:

- Minimum lane widths are displayed for each curb-to-curb width. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.
- Class II Bike Lanes will have a painted buffer, Class IV Cycle Tracks will have a physical buffer.



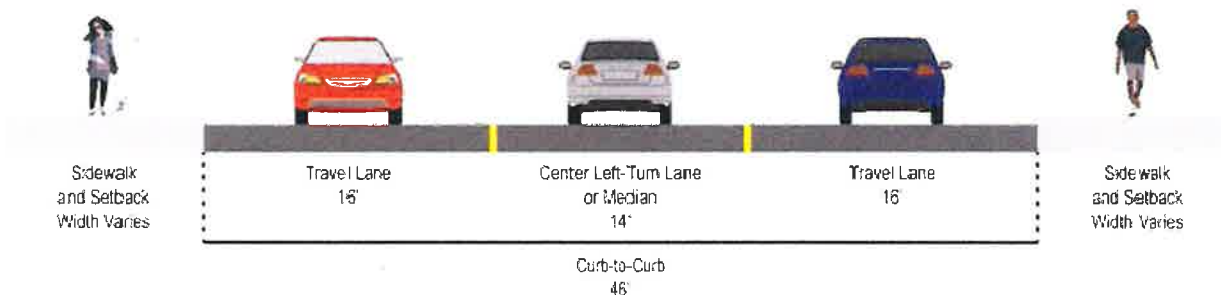
Boulevard

- Madison Street, from Avenue 42 to Fred Waring Drive;
- Madison Street, from Highway 111 to Avenue 52;
- Clinton Street, from Indio Boulevard to Highway 111/Dr. Carreon Boulevard;
- Monroe Street, from Avenue 40 to Avenue 42;
- Monroe Street, from Fred Waring Drive to Avenue 52;
- Jackson Street, from Avenue 41 to Avenue 42;
- Jackson Street, from north of Kenner Avenue to Avenue 52;
- Calhoun Street, from Highway 111 to Avenue 48;
- Golf Center Parkway, from Avenue 42 to Highway 111;
- Van Buren Street, from Indio Boulevard to Avenue 48;
- Dillon Road, north of Avenue 44;
- Avenue 40, from Fifties Way to Monroe Street;
- Avenue 41, from Monroe Street to Jackson Street;
- Varner Road, from western City limit to Madison Street;
- Avenue 42, from Madison Street to Clinton Street;
- Avenue 42, from Jackson Street to Golf Center Parkway;
- Country Club Drive, from western City limit to Avenue 42;
- Avenue 42, from Country Club Drive to Burr Street;
- Indio Boulevard, from Jefferson Street to Avenue 48;
- Avenue 44, from Jackson Street to Golf Center Parkway;
- Miles Avenue, from Dune Palms Road to Monroe Street;
- Highway 111, from Rubidoux Street to Indio Boulevard;
- Dr. Carreon Boulevard, from Clinton Street to Jackson Street;
- Avenue 48, from Jefferson Street to Indio Boulevard;
- Avenue 50, from Jefferson Street to eastern City limit; and
- Avenue 52, from Madison Street to Jackson Street.



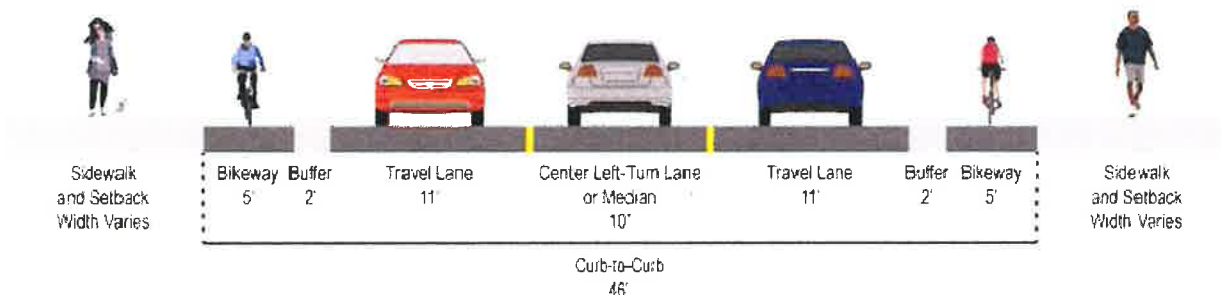
2-Lane Collector (with Median or Center Left-Turn Lane)

2-Lane Collector with Median or Center Left-Turn Lane*



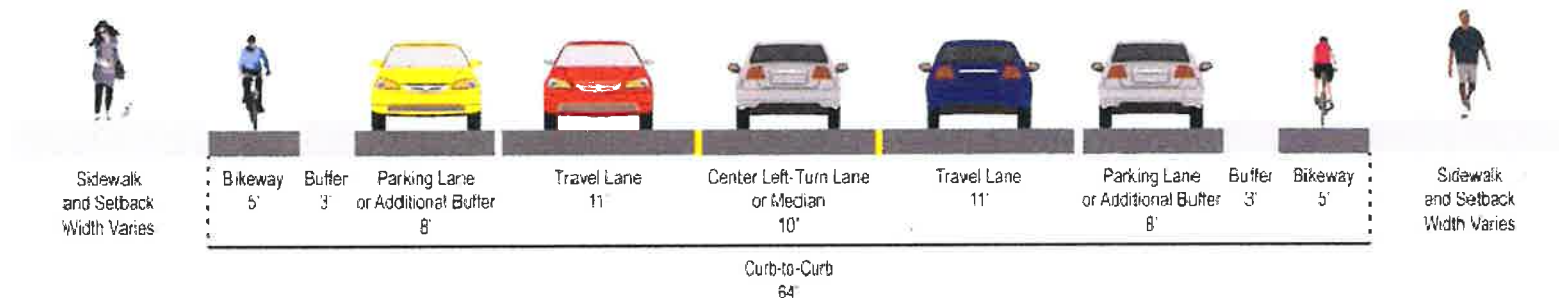
* Minimum lane widths are displayed for a curb-to-curb width of 46'.

2-Lane Collector with Median or Center Left-Turn Lane and Bicycle Facility (< 64' Curb-to-Curb Width*)



* Minimum lane widths are displayed for a curb-to-curb width of 46'. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.

2-Lane Collector with Median or Center Left-Turn Lane and Bicycle Facility (≥ 64' Curb-to-Curb Width*)



* Minimum lane widths are displayed for a curb to curb width of 64'. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.

Notes:

- Minimum lane widths are displayed for each curb-to-curb width. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.
- Class II Bike Lanes will have a painted buffer, Class IV Cycle Tracks will have a physical buffer.



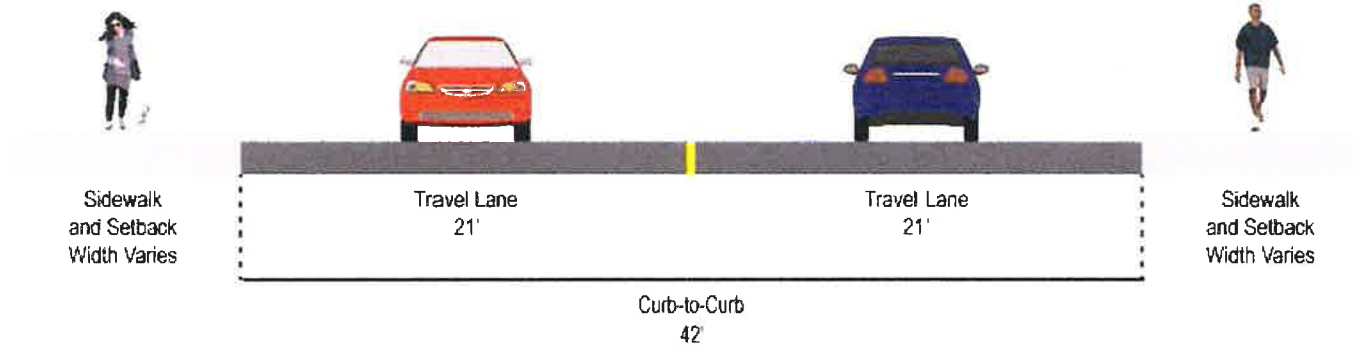
2-Lane Collector (with Median or Center Left-Turn Lane)

- Adams Street, from Coyote Song Way to Varner Road;
- Dune Palms Road, from Miles Avenue to Blackhawk Way;
- Jefferson Street, from Avenue 38 to Avenue 40;
- Madison Street, from Avenue 38 to Avenue 40;
- Madison Street, from Fred Waring Drive to Highway 111;
- Oasis Street, from Indio Boulevard to Avenue 48;
- Gore Street, from Avenue 41 to Avenue 42;
- Spectrum Street, from Avenue 42 to Showcase Parkway;
- Coyote Song Way, from Washington Street to Adams Street;
- Avenue 38, from Adams Street to Madison Street;
- Showcase Parkway, from Spectrum Street to Jackson Street;
- Avenue 44, from Monroe Street to Jackson Street;
- Avenue 44, from Golf Center Parkway to Dillon Road;
- Avenue 45, from Jackson Street to Van Buren Street
- Van Buren Street, from Avenue 45 to Cabazon Road;
- Cabazon Road, from Van Buren Street to Avenue 48;
- Avenue 46, from Madison Street to eastern terminus; and
- Dr. Carreon Blvd, from Jackson Street to Van Buren Street.

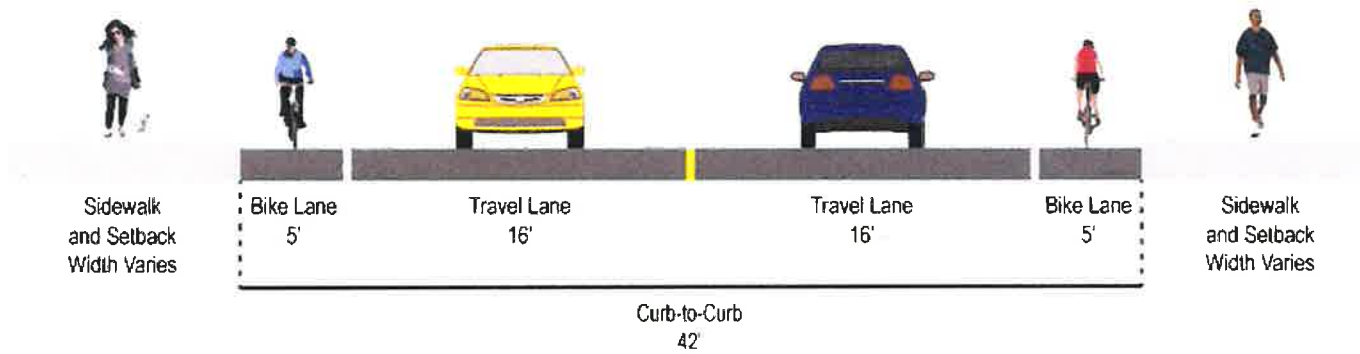


2-Lane Collector

2-Lane Collector with Shoulder, On-Street Parking or Bike Route ($\geq 42'$ Curb to Curb Width)



2-Lane Collector with Bike Lane ($\geq 42'$ Curb to Curb Width)



Notes:

- Minimum lane widths are displayed for each curb-to-curb width. Additional available width should be used to increase buffer width, bikeway width, and/or center left-turn lane/median width.
- Class II Bike Lanes will have a painted buffer, Class IV Cycle Tracks will have a physical buffer.

- Burr Street, from Indio Boulevard to Fred Waring Drive;
- Hoover Avenue, from Fred Waring Drive to Monroe Street;
- Requa Avenue, from Monroe Street to Jackson Street;
- Westward Ho Drive, from Dune Palms Road to Jefferson Street; and
- Avenue 48, from Dillon Road to Harrison Street.



4.1.2 Traffic Modeling

The future year 2035 traffic forecast was developed by *Iteris, Inc.* using the Riverside County Traffic Analysis Model (RivTAM). Traffic modeling methodology, including roadway network and land use assumptions are provided as a separate report by *Iteris*. Future traffic volumes (roadway, freeway, and intersection) for both the Preferred Plan and the No-Project scenario were provided by *Iteris*.

4.1.3 Preferred Plan Network Changes

The Preferred Plan network analysis assumed full roadway classification buildout of adjacent jurisdiction's networks in accordance with their respective adopted Circulation/Mobility Element or General Plan. The following changes are proposed to the existing roadway network under buildout of the Preferred Plan within the City of Indio:

Roadway Widening

Jefferson Street, between Avenue 38 and Avenue 40 - Under the Preferred Plan, this segment will be improved from a 2-lane/4-lane roadway to a 4-lane Boulevard.

Jefferson Street, between Avenue 40 and Varner Road - Under the Preferred Plan, this segment will be improve from a 2-lane/3-lane roadway to a 6-lane Major Arterial.

Madison Street, between Avenue 42 and Indio Boulevard - This segment does not currently exist. Under the Preferred Plan, this segment will be built as a 4-lane Boulevard.

Madison Street, between Indio Boulevard and Fred Waring Drive - Under the Preferred Plan, north of Paludosa Drive this segment will be improved from 2-lanes southbound, one-lane northbound and a center left-turn lane to a 4-lane Boulevard.

Madison Street, between Fred Waring Drive and Miles Avenue - This segment does not currently exist. Under the Preferred Plan, this segment will be built as a 2-lane Collector with a median or center left-turn lane.

Madison Street, between Highway 111 and Avenue 48 – Under the Preferred Plan, north of Sunnyside Drive this segment will be improved from a 3-lane roadway with a center left-turn lane to a 4-lane Boulevard.

Madison Street, Avenue 50 to Avenue 52 - Under the Preferred Plan, this segment will be improved from a 2-lane roadway with a center left-turn lane to a 4-lane Boulevard. Including the signalization of Madison Street & Avenue 50 and Madison Street & Avenue 52.

Monroe Street, between Avenue 42 and Fred Waring Drive - Under the Preferred Plan, this segment will be improved from a 3-lane/4-lane roadway to a 6-lane Major Arterial, including the signalization of the intersection of Monroe Street & Avenue 52.



Gore Street, between Avenue 41 and Avenue 42 - Under the Preferred Plan, this segment will be improved from a 2-lane/4-lane roadway to 2-lane Collector with a median or center left-turn lane.

Jackson Street, between Showcase Parkway to north of Kenner Avenue - Under the Preferred Plan, this segment will be improved from a 2-lane/4-lane roadway with a center left-turn lane to a 6-lane Major Arterial.

Jackson Street, between north of Kenner Avenue to Avenue 44 - Under the Preferred Plan, this segment will be improve from a 2-lane roadway with a center left-turn lane to a 4-lane Boulevard.

Jackson Street, between Avenue 50 and Avenue 52 - Under the Preferred Plan, this segment would remain the same as existing conditions, however signalization is assumed to occur at the intersection of Jackson Street & Avenue 50 and Jackson Street & Avenue 52.

Golf Center Parkway, between Avenue 45 and Highway 111 - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 4-lane Boulevard.

Van Buren Street, between Indio Blvd and Avenue 48 - Under the Preferred Plan, this segment will be improved from a 2-lane roadway with a center left-turn lane to a 4-lane Boulevard.

Dillon Road, between Fargo Canyon Road and Avenue 44 - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 4-lane Boulevard, including signalization of the intersection of Dillon Road & Avenue 44.

Avenue 38, between Adams Street and Dune Palms Road - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 2-lane Collector with a median or center left-turn lane.

Avenue 38, between Talavera Boulevard/Burr Street and Madison Street - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 2-lane Collector with a median or center left-turn lane.

Avenue 40, between Fifties Way to Jefferson Street - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 4-lane Boulevard.

Varner Road, between Jefferson Street and Madison Street - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 4-lane Boulevard.

Avenue 42, between Madison Street and Gore Street - Under the Preferred Plan, this segment will be improved from a 2-lane/4-lane roadway to a 4-lane Boulevard.

Showcase Parkway, between Spectrum Street and Jackson Street - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 2-lane Collector with a median or center left-turn lane.



Avenue 44, between Jackson Street and Golf Center Parkway – Under the Preferred Plan, this segment will be improved from a 4-lane undivided roadway / 2-lane roadway to a 4-lane Boulevard.

Avenue 44, between Golf Center Parkway and Dillon Road - Under the Preferred Plan, this segment will be improved from a 2-lane roadway to a 2-lane Collector with a median or center left-turn lane.

Highway 111, between Madison Street and Rubidoux Street - Under the Preferred Plan, this segment will be improved from a 4-lane roadway to a 6-lane Major Arterial.

Avenue 48, between Calhoun Street and Van Buren Street - Under the Preferred Plan, this segment will be improved from a 3-lane roadway with a center left-turn lane to a 4-lane Boulevard, including the signalization of the intersection of Oasis Street & Avenue 48.

Avenue 50, between Jefferson Street and Van Buren Street - Under the Preferred Plan, this segment will be improved from a 2-lane/3-lane roadway with a median or center left-turn lane to a 4-lane Boulevard.

Road Diet (reduction of the # of travel lanes)

Adams Street, between northern terminus and Varner Road - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 4-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be repurposed for alternative modes of transportation.

Oasis Street, between Indio Boulevard and Highway 111 - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 4-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be repurposed for increased on-street parking.

Oasis Street, between Highway 111 and Hartford Avenue - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 4-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be repurposed for alternative modes of transportation.

Avenue 44, between Monroe Street and Jackson Street - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 4-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be repurposed for alternative modes of transportation. The center left-turn lane and/or median will improve access to adjacent local streets and driveways.

Dr. Carreon Boulevard, between Jackson Street and Calhoun Street - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 4-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be



repurposed for alternative modes of transportation. The center left-turn lane and/or median will improve access to adjacent local streets and driveways.

Coyote Song Way, between Washington Street and Eastern Terminus - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 4-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be repurposed for alternative modes of transportation.

Madison Street, between Avenue 38 and Avenue 40 - Under the Preferred Plan, this roadway segment has excess capacity will be downgraded from a 3-lane roadway to a 2-lane Collector with a median or center left-turn lane. The additional pavement width will be repurposed for alternative modes of transportation. The center left-turn lane and/or median will improve access to adjacent local streets and driveways.

New Road Segments

Adams Street, between Coyote Song way and Adams Street northern terminus - Under the Preferred Plan, this roadway segment will be built as a 2-lane Collector with a median or center left-turn lane.

Madison Street, between Avenue 42 and Indio Boulevard - Under the Preferred Plan, this roadway segment will be built as a 4-lane Boulevard.

Madison Street, between Fred Waring Drive and Miles Avenue - Under the Preferred Plan, this roadway segment will be built as a 2-lane Collector with a median or center left-turn lane.

Coyote Song Way, Coyote Song Way eastern Terminus and future northern terminus of Adams Street - Under the Preferred Plan, this roadway segment will be built as a 2-lane Collector with a median or center left-turn lane.

Figure 4-1 displays the proposed roadway classifications under buildout of the Preferred Plan.

In addition to the roadway network changes, an additional mixed-flow lane would be constructed in each direction along Interstate 10 between Monterey Ave to Dillon Rd as indicated in the SCAG 2012 – 2035 Regional Transportation Plan / Sustainable Community Strategy (April 2012).