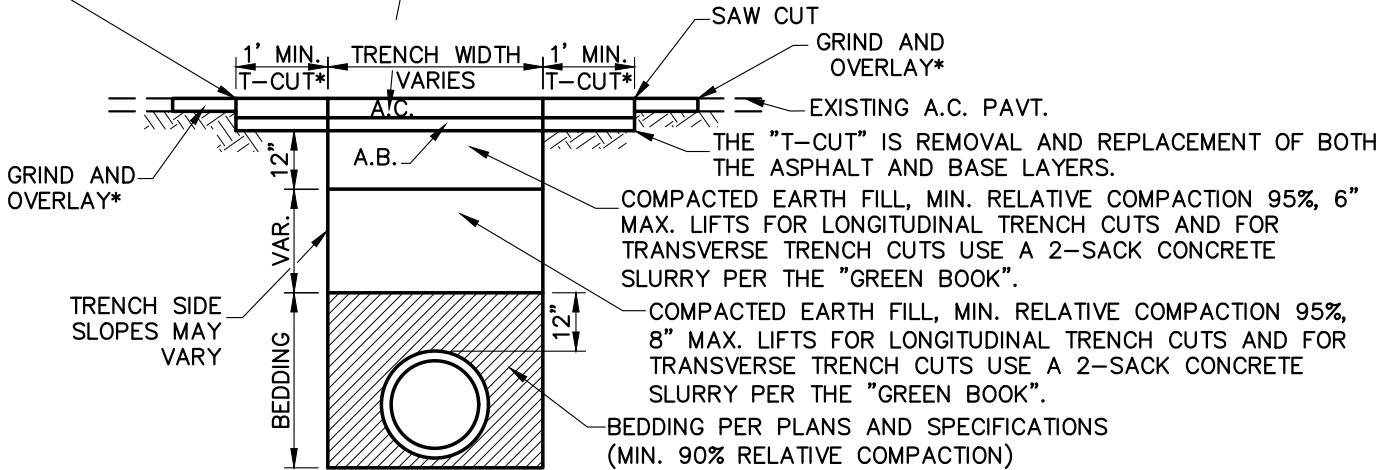


**STREET STRUCTURAL SECTION REQUIRED
PER STANDARD PLAN NUMBER 171**

JOIN EXISTING A.C. PAVT.
WITH SMOOTH BUTT JOINT
(NO FEATHERING)

NOTE: IF EITHER THE EXISTING A.C. OR A.B. IS THICKER THAN PER
STANDARD PLAN NUMBER 171, THEN THE EXISTING A.C. OR A.B.
THICKNESS SHALL BE USED.



"T-TRENCH" DETAIL
NOT TO SCALE

* AS REQUIRED (SEE BELOW)

LONGITUDINAL TRENCH CUTS IN DRIVING LANES

- 0" TO 12" WIDE – REPAIR TRENCH WIDTH, PLUS 1.5" GRIND AND OVERLAY FROM LANE LINE TO LANE LINE.
- > 12" WIDE – REPAIR TRENCH WIDTH PLUS AN ADDITIONAL 1' "T-CUT" ON BOTH SIDES, PLUS 1.5" GRIND AND OVERLAY FROM LANE LINE TO LANE LINE.

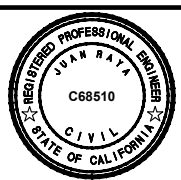
TRANSVERSE TRENCH CUTS (PERPENDICULAR TO CENTERLINE OF ROAD)

- 0" TO 12" WIDE – REPAIR TRENCH WIDTH ONLY.
- 12" TO 24" WIDE – REPAIR TRENCH WIDTH PLUS AN ADDITIONAL 1' "T-CUT" ON BOTH SIDES.
- >24" WIDE – REPAIR TRENCH WIDTH PLUS AN ADDITIONAL 1' "T-CUT" ON BOTH SIDES, PLUS 1.5" GRIND AND OVERLAY FOR AN ADDITIONAL 3' ON BOTH SIDES.

SHOULDER TRENCH CUTS

AT EDGE OF GUTTER – REPAIR TRENCH WIDTH ONLY (NO "T-TRENCH").
OFF EDGE OF GUTTER – NO LESS THAN 2' "FLOATER" BETWEEN TRENCH AND GUTTER.
SLURRY OR FOG SEAL MAY BE REQUIRED UNLESS THE ROADWAY IS A DESIGNATED BIKE ROUTE, WHICH WOULD REQUIRE A 1.5" THICK GRIND AND OVERLAY FROM EDGE OF GUTTER TO LANE LINE.

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Juan Raza 05/18/16
 JUAN RAZA, P.E. DATE
 CITY ENGINEER
 RCE 68510

REV.	DRAWN BY:	APP'D:	DATE:



**TRENCH
REPAIR
DETAIL**

STANDARD PLAN No.: **172**
 SHEET 1 OF 2

MAY 2016

SPECIAL NOTES FOR TRENCH REPAIR DETAIL

A. ASPHALT CONCRETE (A.C.):

1. A.C. SURFACE LAYER, OR CAP, SHALL BE 1/2-INCH MAXIMUM, MEDIUM, PG 70-10 MATERIAL PER CALTRANS SPECIFICATIONS.
2. A.C. BASE OR BOTTOM, LAYER(S) SHALL BE 3/4-INCH MAXIMUM, MEDIUM, PG 70-10 MATERIAL PER CALTRANS SPECIFICATIONS.
3. MAXIMUM A.C. LIFT THICKNESS = 3.0".
4. ALL A.C. BASE LAYERS AND ALL EDGES TO RECEIVE HEAVY TACK COAT.
5. WHEN SURFACE WIDTH OF A.C. TRENCH TO BE PAVED IS 10', OR WIDER, A BARBER-GREENE PAVER, OR EQUAL, SHALL BE USED.

B. AGGREGATE BASE (A.B.):

1. AGGREGATE BASE (A.B.) SHALL BE CLASS II A.B. PER CALTRANS SPECIFICATIONS.
2. ALL A.B. AS PART OF STRUCTURAL SECTION SHALL BE COMPACTED TO 95% RELATIVE COMPACTION.
3. A.B. MAY BE USED AS TRENCH BACKFILL; HOWEVER, THE PORTION THAT IS PART OF THE STREET STRUCTURAL SECTION AND THE ADDITIONAL TOP 12" OF A.B. SHALL BE COMPACTED TO 95% RELATIVE COMPACTION.

C. CONCRETE, 2 SACK SLURRY:

1. FOR TRENCH WIDTHS LESS THAN 12", CONCRETE SLURRY MAY BE BROUGHT TO 4" MIN. BELOW TOP OF A.C. SURFACE.
2. FOR TRENCH WIDTHS GREATER THAN 12", CONCRETE SLURRY MAY BE BROUGHT TO BOTTOM OF A.B. LAYER (I.E. NEED COMPLETE A.B. AND A.C. LAYERS).

D. TIMING OF FINAL A.C. LAYER (OR CAP):

1. THE A.C. SURFACE LAYER, OR CAP, SHALL BE PLACED THE SAME DAY AS THE BASE A.C. UNLESS SPECIFICALLY APPROVED OTHERWISE BY CITY ENGINEER.
2. IF THE A.C. SURFACE LAYER IS NOT PLACED THE SAME DAY AS THE BASE A.C. LAYER(S), THEN,
 - NO "LIPS" SHALL BE LEFT OPEN TO TRAFFIC ("LIPS" SHALL BE RAMPED, TRENCH CAPPED WITH COLD-MIX, OR OTHERWISE ADDRESSED TO SATISFACTION OF THE CITY ENGINEER).
 - THE A.C. SURFACE LAYER SHALL BE PLACED AS SOON AS POSSIBLE, BUT IN NO CASE LONGER THAN 14 CALENDAR DAYS, AFTER THE BASE A.C. LAYER(S) ARE PLACED.

E. SPECIAL SITUATIONS:

1. THE CITY ENGINEER MAY REQUIRE ADDITIONAL REQUIREMENTS FOR TRENCH REPAIR FOR SPECIAL SITUATIONS.
2. SPECIAL SITUATIONS SHALL INCLUDE, BUT NOT BE LIMITED TO:
 - 1) MULTIPLE TRENCHES IN SAME AREA,
 - 2) TRENCHING ON ROAD CAPPED WITH RUBBER ASPHALT, AND
 - 3) TRENCHING IN ROADWAY THAT WAS PAVED WITHIN THE PAST 3 YEARS (THIS WILL ONLY BE ALLOWED IN LIMITED CIRCUMSTANCES).

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Juan Raya 05/18/16
DATE

JUAN RAYA, P.E.
CITY ENGINEER
RCE 68510

REV.	DRAWN BY:	APP'D:	DATE:



**SPECIAL NOTES
FOR
TRENCH REPAIR**

STANDARD PLAN No.: **172**
SHEET 2 OF 2

MAY 2016