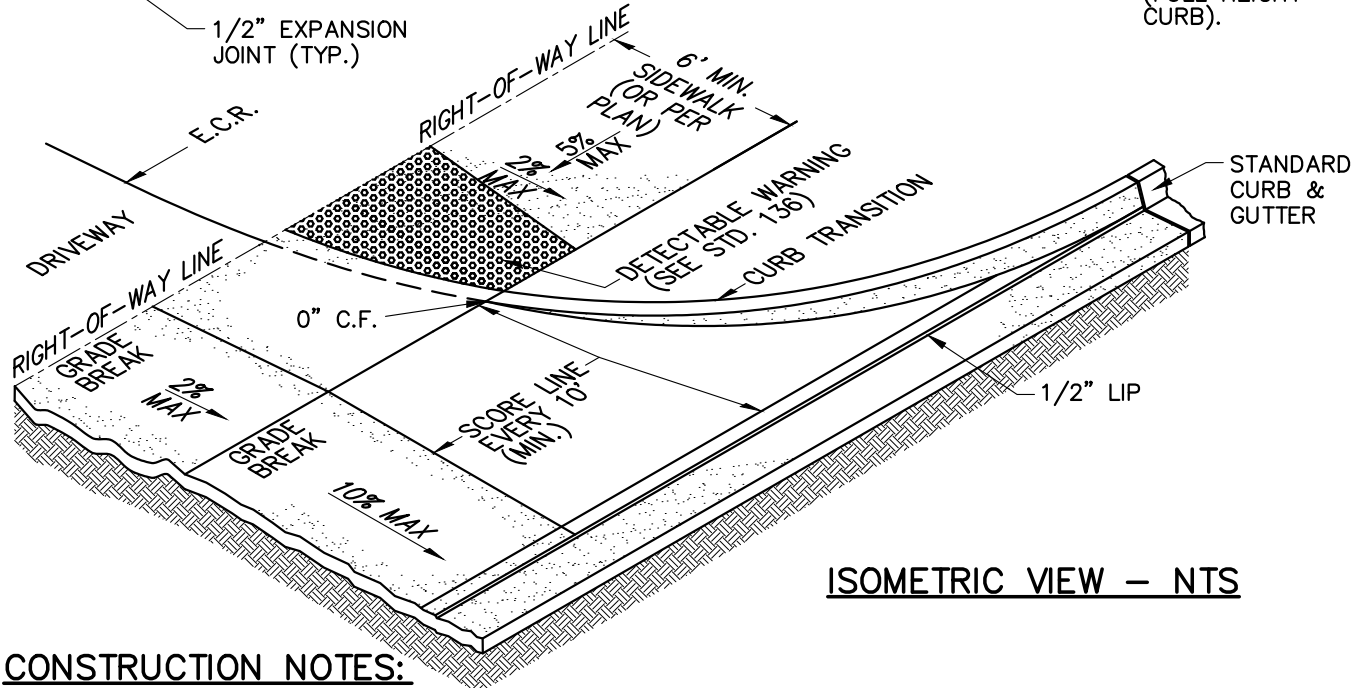


**PLAN VIEW**



**ISOMETRIC VIEW - NTS**

**CONSTRUCTION NOTES:**

COMMERCIAL DRIVEWAY APPROACH DETAILS TYPES A AND B:

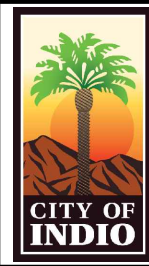
1. DRIVEWAY APPROACHES INCLUDE ALL IMPROVEMENTS SHOWN ON DETAILS FROM BEGINNING OF APPROACH TO END OF APPROACH WITHIN THE PUBLIC R/W.
2. ALL CONCRETE TO BE CLASS 560-C-3250. DRIVEWAYS TO BE 8" (MINIMUM) CONCRETE ON 6" (MINIMUM) CLASS 2 AGGREGATE BASE.
3. THE TOP 12-INCHES OF SUBGRADE SHALL BE 95% (MINIMUM) RELATIVE COMPACTION.
4. EXPANSION JOINTS ARE REQUIRED AT THE BEGINNING AND END OF THE DRIVEWAY APPROACH AND AT THE RIGHT OF WAY. WEAKENED PLANE JOINTS (1.5" DEEP) ARE REQUIRED AT 10' O.C. (MAX.) PERPENDICULAR TO STREET FLOWLINE FOR REMAINDER OF DRIVEWAY APPROACH. SIDEWALK AND CURBS WITHIN DRIVEWAY APPROACH SHALL BE SCORED PER THEIR RESPECTIVE DETAILS.
5. ALL CONCRETE TO RECEIVE A MEDIUM BROOM FINISH.
6. IT IS RECOMMENDED THAT THE "DRIVEWAY APPROACH" BE BUILT PRIOR TO THE "DRIVEWAY" AS THE HEIGHT OF THE IMPROVEMENTS AT THE RIGHT-OF-WAY LINE WILL BE DETERMINED BY THE "DRIVEWAY APPROACH".

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*Juan Raza* 05/18/16  
 JUAN RAZA, P.E.  
 CITY ENGINEER  
 RCE 68510

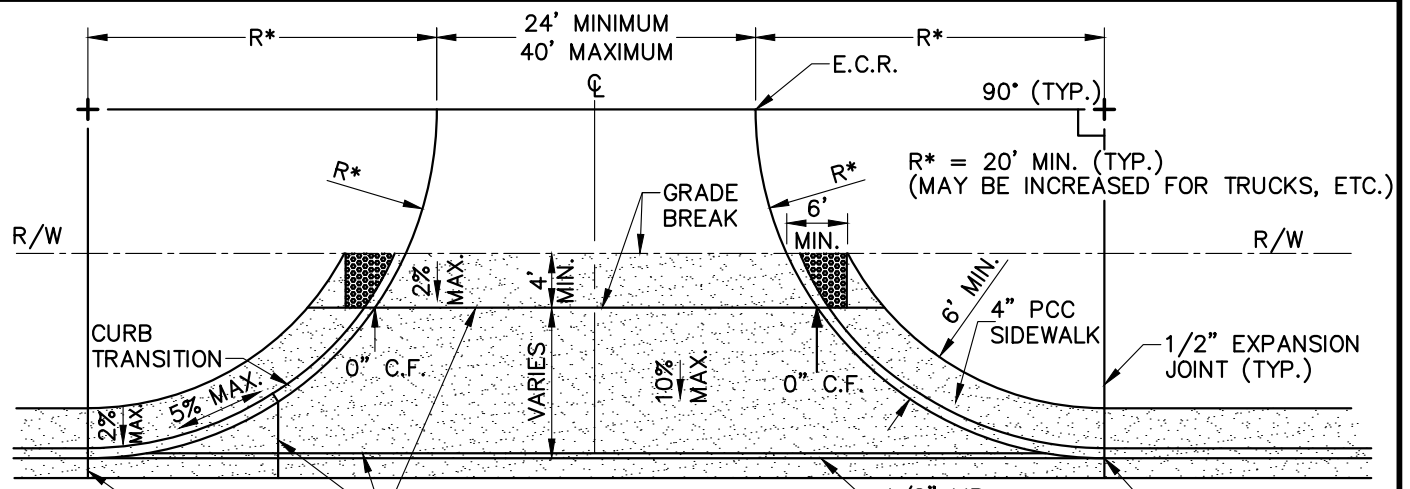
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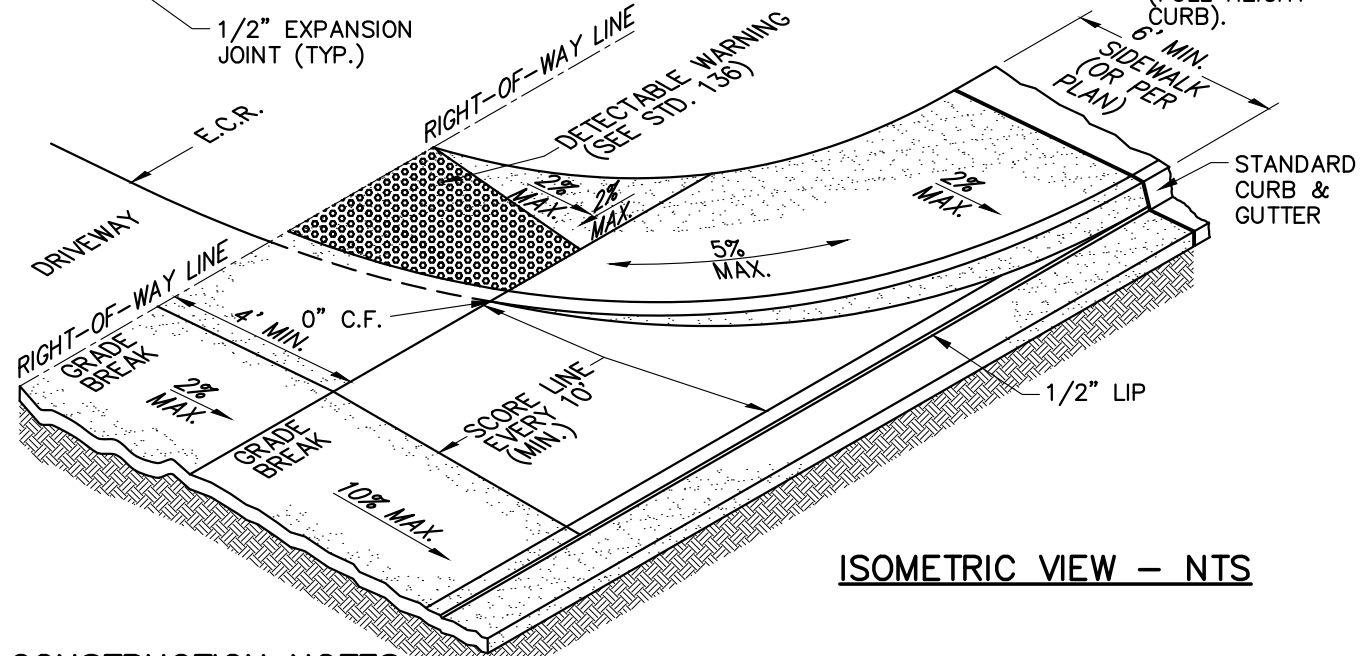
**COMMERCIAL DRIVEWAY APPROACH (TYPE A) (SIDEWALK NOT @ CURB)**

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**PLAN VIEW**



**ISOMETRIC VIEW – NTS**

**CONSTRUCTION NOTES:**

COMMERCIAL DRIVEWAY APPROACH DETAILS TYPES A AND B:

1. DRIVEWAY APPROACHES INCLUDE ALL IMPROVEMENTS SHOWN ON DETAILS FROM BEGINNING OF APPROACH TO END OF APPROACH WITHIN THE PUBLIC R/W.
2. ALL CONCRETE TO BE CLASS 560-C-3250. DRIVEWAYS TO BE 8" (MINIMUM) CONCRETE ON 6" (MINIMUM) CLASS 2 AGGREGATE BASE.
3. THE TOP 12-INCHES OF SUBGRADE SHALL BE 95% (MINIMUM) RELATIVE COMPACTION.
4. EXPANSION JOINTS ARE REQUIRED AT THE BEGINNING AND END OF THE DRIVEWAY APPROACH AND AT THE RIGHT OF WAY. WEAKENED PLANE JOINTS (1.5" DEEP) ARE REQUIRED AT 10' O.C. (MAX.) PERPENDICULAR TO STREET FLOWLINE FOR REMAINDER OF DRIVEWAY APPROACH. SIDEWALK AND CURBS WITHIN DRIVEWAY APPROACH SHALL BE SCORED PER THEIR RESPECTIVE DETAILS.
5. ALL CONCRETE TO RECEIVE A MEDIUM BROOM FINISH.
6. IT IS RECOMMENDED THAT THE "DRIVEWAY APPROACH" BE BUILT PRIOR TO THE "DRIVEWAY" AS THE HEIGHT OF THE IMPROVEMENTS AT THE RIGHT-OF-WAY LINE WILL BE DETERMINED BY THE "DRIVEWAY APPROACH".

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*Juan Raza* 05/18/16  
 JUAN RAZA, P.E.  
 CITY ENGINEER  
 RCE 68510

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**COMMERCIAL DRIVEWAY APPROACH (TYPE B) (SIDEWALK @ CURB)**

MAY 2016

STANDARD PLAN No.: **130**  
 SHEET 2 OF 2