

3 Interim Development Standards



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Chapter 3 contains the Interim Development Standards for Downtown, which are intended to implement the envisioned physical design, character, and uses of all buildings. The standards are guided by the following objectives:

1. Generate appropriately-scaled blocks and buildings for the intended physical character of Downtown Indio.
2. Protect the character of adjacent neighborhoods.
3. Provide neighborhood main streets for vibrant social and commercial focal points within walking distance of many homes and transit.
4. Provide diverse and high-quality housing choices.
5. Provide and manage area-serving parking facilities that serve to create a park-once pedestrian district.



Part I Overview

Part I provides an overview of **Chapter 3**. As the majority of this Chapter consists of interim development standards, Part I provides the following introductory sections:

- General Provisions
- Glossary
- Zoning

These sections are intended to be a starting point and reference for all users of this Chapter, as it provides the foundation upon which the rest of the Chapter depends.

3.1.1 Introduction

The Land Use and Urban Design Element of the current General Plan describes a future that is comfortably walkable, amenity-rich, and well-connected. The city-wide vision includes complete, compact neighborhoods, human-scaled streets that are framed by buildings with active and interesting frontages, and a network of high-quality open spaces that weave through the city and provide a range of mobility options. The Plan calls for a focus on concentrating limited staff time, City resources, and developer capital into key subareas. One of the high-priority areas specified for such focus is the Downtown. It is the purpose of the Downtown Specific Plan, and especially of these standards, to help ensure that this vision is implemented.

These Interim Development Standards seek to guide private development efforts and public improvements so that as the Downtown grows, it grows in a way that is physically predictable, and consistent with the goals and policies of the General Plan.

3.1.2 Applicability

The Interim Development Standards regulate land use and the physical form of development for all parcels identified in **Figure 3.3: Zoning Map**. The two new zones established herein replace the zoning for all property and uses within its boundaries.

A. Existing Requirements. This Chapter replaces currently-adopted zoning regulations for parcels in the Plan area, but relies on many of the land use and procedural requirements of the Indio Municipal Code (IMC). In cases where this Chapter is silent, the requirements of the IMC shall apply; in cases where there is conflict between the requirements of this Chapter and those of the IMC, the requirements of this Chapter shall apply.

B. Existing Structures and Uses. Permitted structures and uses that do not conform to the standards of this Chapter on the date of its adoption shall be deemed to be legal nonconforming. While all additions and changes in use are subject to all applicable standards within this Chapter, full compliance shall be required of the whole parcel when a total project valuation is greater than fifty percent of the current market value of all existing buildings or structures on the parcel. The provisions of IMC § 159.805-819 as amended shall apply to all parcels within the Plan area.

3.1.3 Rules of Construction

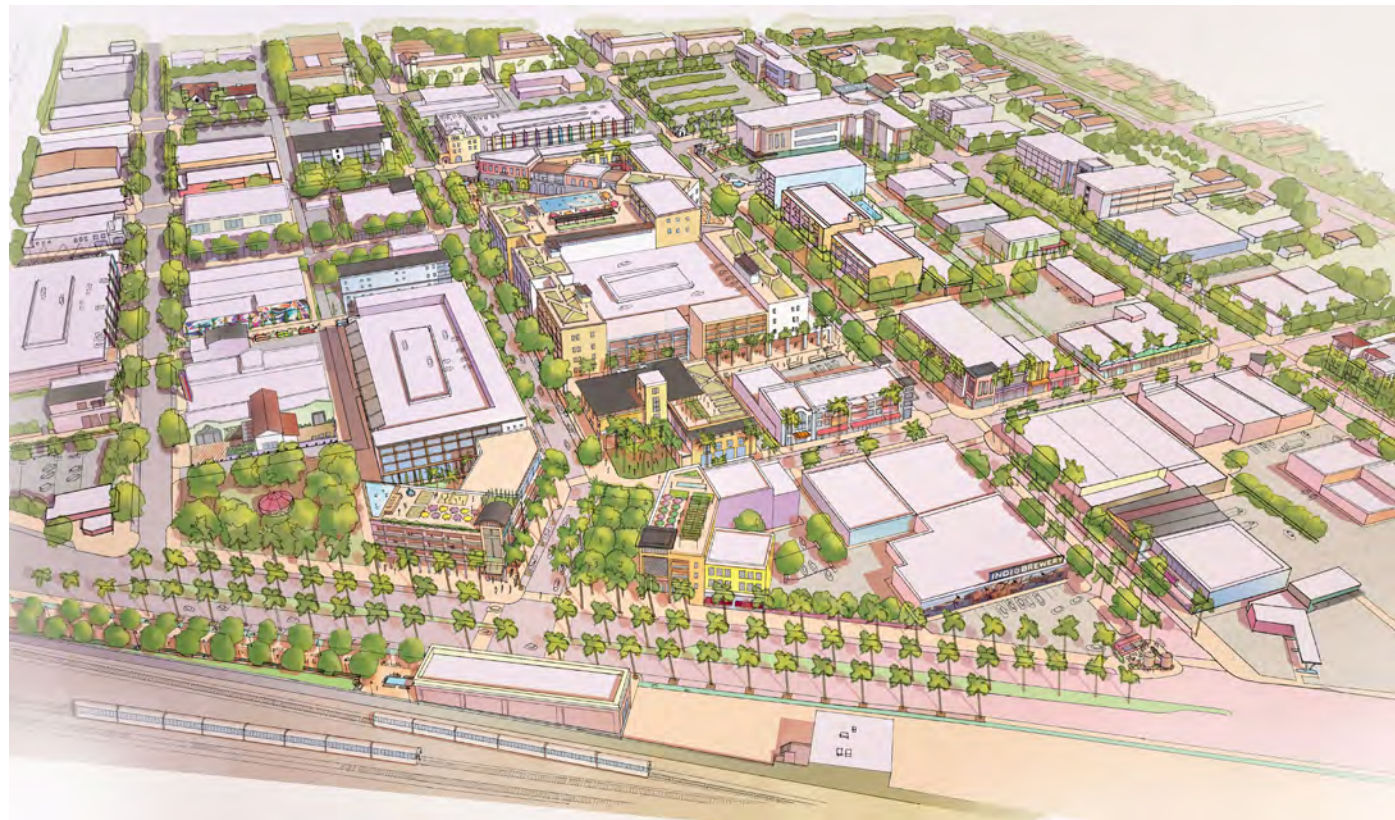
The following general rules of construction apply to the text of this Chapter. In case of ambiguity, the Director shall determine the appropriate application of standards based on the vision for Downtown Indio described in **Chapter 1**.

A. Definitions. Whenever a defined word appears in this Chapter, its meaning is as set forth in this article. Words not defined in this Chapter are interpreted in accord with their dictionary meaning and customary usage. In cases of ambiguity, the Director shall determine the appropriate meaning.

B. Illustrations. In case of any difference of meaning or implication between the text of any provision and any illustration or photograph, the text shall control, unless the intent of the standard is clearly otherwise.

C. Terminology. "Shall" is prescriptive; "should" is directive; "may" is permissive.

D. Uncertainty. In cases of uncertainty regarding the application of any of these Interim Development Standards, the Director shall have the authority to make a determination.



3.1.4 Design Review

A. General. In addition to all other relevant requirements established in the Indio Municipal Code, all projects involving new construction shall be subject to the Design Review process established herein.

B. Design Review. All new development proposals and redevelopment proposals including but not limited to landscape alteration, parking modifications, building additions or deletions, and façade modifications shall be subject to Design Review by the Planning Commission.

C. Design Review Fee. All new development and redevelopment proposals shall be subject to Design Review fees established by the City Council.

D. Submittal Requirements. The following items shall be included in the application package at time of submittal.

- Planning Application
- Planning Fee
- Site Plan
- Floor Plan (when applicable)
- All elevation drawings
- Landscape Plan
- Photometric Plan
- Public Improvements Plan (when applicable)
- Rooftop Mechanical Screening Plan (when applicable)
- View Corridor Plan (applicable when buildings are 4 stories or greater in height)
- All applicable items additionally required in **Table 3.1.4**.

Based on the project type for which Design Review approval is sought, the Director shall have the authority to determine that one or more of the aforementioned submittal items is not required to be submitted and shall notify the applicant in writing of this determination.

E. Process. Upon receipt of a complete application, the Director shall review the project plans for consistency with the requirements of the Interim Development Standards of this Chapter, and all applicable findings required in **Table 3.1.4**:

1. Project Consultation. Applicants shall meet with the Planning Commission prior to formal submittal of the project application. At least two weeks before the scheduled meeting, the applicant shall submit to the Director a conceptual site and landscape plan, conceptual building elevations, and a written narrative containing a project description, how the proposed project meets the Interim Development Standards and their intent, and any anticipated deviations that will be sought.
2. Project Submittal. After the project consultation with the Planning Commission, the applicant may submit the complete Design Review application to the Planning Department.
3. Planning Commission Design Review.
 - a. Hearing. The Director shall agendaize the application no later than 90 calendar days from the date that the application is deemed complete, provided that the Director shall have the discretion to establish an alternative processing timeline following notice and consultation with the applicant.
 - b. Notice of Decision. The Planning Commission shall issue a written Notice of Decision approving, approving with conditions or denying the Planning Commission Design Review request. The Notice of Decision shall include all applicable written findings and shall include conditions of approval, as applicable.
4. Appeal. All Planning Commission decisions shall be appealable to the City Council.

Table 3.1.4 Project-Specific Submittal Items and Required Findings

Within Project Scope	Additional Required Submittal Items	Required Findings
Reconfiguration of alley (Section 3.6.1.A.2).	A map of entire block, showing how the rear access connects to the rear access of all other parcels on the block, and to the street.	<ul style="list-style-type: none"> • That the configuration is feasible for all parcels on the block; • That the configuration does not preclude its legal and feasible continuation of a rear vehicular right of way to at least one other side of the block; • That the configuration is feasible for all service activities, including waste pickup and the delivery of goods, for all parcels on the block; • That the proposed connection(s) to surrounding streets are at least 100' from the nearest intersection (measured from curb face).
A request for Community Benefits Height Bonus pursuant to Section 3.1.6 .		<ul style="list-style-type: none"> • No additional findings required.
A reduction, removal, or substantial alteration of the design of public improvements.		<ul style="list-style-type: none"> • No additional findings required.
New construction on > 400' of street frontage on the same block face.	Colored renderings of the project from all sides visible from a public space. The renderings shall include the buildings on either side of the project site and shall serve to demonstrate the way in which the proposed project relates to the existing character of the street and shapes the public realm.	<ul style="list-style-type: none"> • The building utilizes exterior materials intuitively, with those that are traditionally heavier below those that are traditionally lighter; • The architect has attempted to imitate a collection of narrower buildings, each 100' or less in width; • The design is simple, with each "narrower building" represented by its own façade composition, each internally coherent and lacking gratuitous complication.
A multi-level public or private parking structure.		<ul style="list-style-type: none"> • The architectural quality of the structure maintains a similar level of detail and pattern of openings as other buildings in the vicinity; • The architect has attempted to design the structure in such a way that it does not look like a garage; • A 5-foot setback is provided, within which is landscaping of the same quality as elsewhere on the property.
Civic Building (Section 3.2: Glossary).		<ul style="list-style-type: none"> • The building is intended to be frequently used, generating daily foot-traffic; • The primary entrance is prominent, conspicuous, and oriented toward the street or public open space; • The primary façade contains local symmetries (such as those created by the rhythm and frequency of openings and architectural elements) that contribute to a balanced composition of the whole façade; • The structure is made of high-quality materials; • No fences or walls greater than 3.5' are between the façade and the public right of way or open space; • The building does not contain long stretches of blank walls.

3.1.5 Deviations from Standards

A. Applicability. The intent of the interim standards of this Chapter is to generate the vision for the Plan area through a streamlined review process. However, it is possible that certain design proposals can vary from the prescribed form while still meeting the intent of the standards. A granted deviation shall supersede the normally-required development standards.

B. Authority. The Planning Commission shall have the authority to grant deviations from the standards listed in **Table 3.1.5** in excess of the extent specified.

C. Additional Required Findings for Deviations. In addition to all applicable findings specified in **Table 3.1.5**, the following findings shall be required for all approvals deviation approvals:

1. The overall intent of the development standard is still being met; and
2. The project contributes to the overall vision for Downtown Indio as described in **Chapter 1** and conceptually illustrated in **Chapter 2**.

Table 3.1.5 Deviations from Standards

Standard		Required Finding(s)
1	Building Placement (3.6)	
1a	Front street and Side street Build-to line (Table 3.6)	<ul style="list-style-type: none"> • The building(s) help(s) define and provide appropriate enclosure to the public realm. • The design is harmonious with the existing built context.
1b	Min. buildout percentage for space not utilized for parking (Table 3.6)	
	Min. buildout percentage for space to be utilized for parking, see permitted deviations specific to surface parking below (4b)	<ul style="list-style-type: none"> • See required findings under 4b below.
2	Building Height & Massing (3.7)	
2a	Ground story depth (Table 3.7)	<ul style="list-style-type: none"> • The depth is sufficient as leasable space for residential, retail, and/or service uses.
2b	Building length (Table 3.7)	<ul style="list-style-type: none"> • Exceptional architectural style and quality of materials compensates for a lack of variety.
3	Private Frontage (3.8)	
3a	Frequency of entrances (3.8.1.B)	<ul style="list-style-type: none"> • Exceptional architectural style and quality of materials create visual interest that compensates for a lack of windows and doors.
3b	Frequency of openings (3.8.1.D.2)	
3c	Façade coverage (3.8.1.D.4)	
3d	Wall height (3.8.1.E.1)	<ul style="list-style-type: none"> • The intent of an active and interesting ground floor environment is still met.
3e	Shopfront requirement (3.8.1.H.1 & 2)	
3f	Width of shopfront bay / shopfront recess (3.8.5.A)	
3g	Any other frontage variation (3.8.4 & 5)	
4	Parking & Parking Facilities (3.10)	
4a	Parking access (any standard) (3.10.2.B)	<ul style="list-style-type: none"> • Measures are taken to mitigate visual impact.
4b	Surface Parking (3.10.3.A.2.c)	
4c	Parking lining (3.10.4)	<ul style="list-style-type: none"> • The architectural quality of the structure maintains a similar level of detail and pattern of openings as other buildings in the vicinity; • The architect has attempted to design the structure in such a way that it does not look like a garage; • A 5-foot setback is provided, within which is landscaping of the same quality as elsewhere on the property.
5	Signs (3.11)	
5a	(Any standard)	<ul style="list-style-type: none"> • Unless along Indio Boulevard, Jackson Street, or Highway 111, the sign is not auto-oriented in nature; • The sign is appropriately scaled to the size of the building and business; • The sign enhances the aesthetic quality of the public realm consistent with the vision described in Chapter 1.

3.1.6 Community Benefits Height Bonus

A. Intent. Certain desired elements of the Downtown environment are not likely to be provided by the market alone. In order to generate some of these elements, the Interim Development Standards include the opportunity for the allowance of additional stories in exchange for specified community benefits.

B. Agreement required. To take advantage of the opportunity for an increase in height limit, the developer is required to enter into a binding agreement with the City that specifies the public benefits that will be provided, and the period during which the entitlement will be available to the developer. Any project utilizing this opportunity shall be subject to Downtown Design Review, in which it shall be ensured that the proposed community benefit is adequate, a coherent part of the overall development, and contributes to the vision for Downtown Indio as described in *Chapter 1*.

C. Story Bonus. In exchange for the eligible public benefit(s) per the recorded agreement, the City shall allow the same developer that is providing the benefit to construct additional complete floors of occupiable space per *Table 3.1.6: Community Benefits Height Bonus*.

D. Eligible Community Benefits. Listed in *Table 3.1.6* are elements that would be beneficial to Downtown Indio. Additional benefits may be determined in the future by the Community Development Director, or may be determined during project review based on evolving conditions and/or local needs. Unless stated otherwise, all public benefits must be provided in the Plan area in accordance with the provisions of this Chapter and recorded agreement. The City retains discretion to accept or require an in-lieu monetary contribution to construct the benefit.

Table 3.1.6 Community Benefits Height Bonus

Community Benefit	Story Limit	Conditions
Public Open Space	5	Public pocket park, green, or plaza designed per the standards of <i>Section 4.4</i> in the locations shown or in addition to those shown in <i>Figure 4.4</i> . One of each of these specified open space types is eligible for this bonus per block.
Paseo	5	Pedestrian walkway designed per the standards of <i>Section 4.4.B</i> in a location shown or in addition to those shown in <i>Figure 4.4</i> . The paseo shall be no closer than 215 feet to another paseo or parallel right of way, and shall connect one street to another.
Terrace	5	May occur on rooftop, above podium, or other comparable open-air above-grade space; must either built and maintained for frequent use; minimum 2,000 square feet.
End-of-trip Facility	5	Must include secure bicycle parking, lockers, changing room(s), and shower(s).
Shared Parking Facility	5	Must contain at least 50 parking spaces not assigned to any specific business or use.
Public Frontage and Street Improvements (see <i>Sections 4.2 & 4.3</i>)	5 or 6	5 stories: improvements valued between 1.5X and 3X of that which project applicant is otherwise responsible for. 6 stories: improvements valued at 3X or more than that which project applicant is otherwise responsible for.
Convertible Shared Parking Facility	6	Must contain at least 50 parking spaces not assigned to any specific business or use, and be designed to be entirely convertible to occupiable space.
Student Housing	6	At least 25 dwelling units specifically built and marketed for students of the College of the Desert.
Hotel	6	Either freestanding or part of a larger project. Minimum 50 rooms.



Rooftop terrace as a residential amenity.



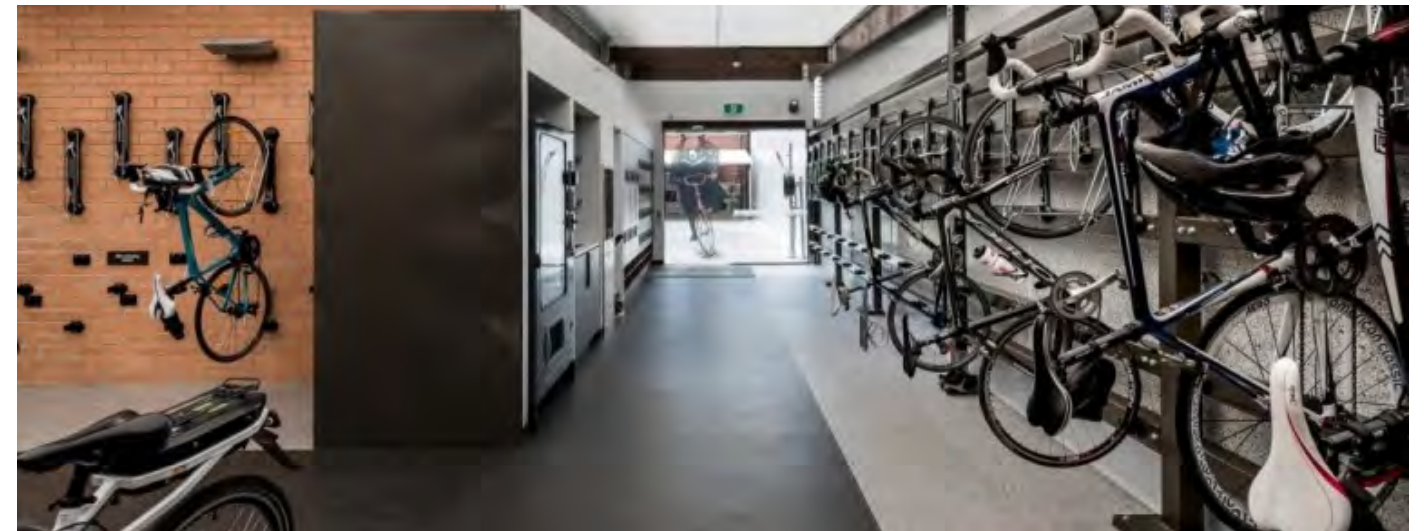
Paseos not only provide mid-block connections, but can create new spaces onto which businesses may front.



A boutique hotel would be a welcome addition to Downtown.



Convertible parking facilities allow for flexible adaptation.



End-of-trip facilities can make commuting by bike a viable option for many employees.

3.2.1 Applicability

The following additions are supplemental to those in the Indio Municipal Code. When in conflict, the definitions herein shall prevail. For definitions specific to uses, see *Section 3.5.2: Use Definitions*.

Access Drive. A private right of way primarily intended as vehicular access to on-site parking or loading.

Arcade. A façade with a ground floor colonnade supporting the upper stories of the building, intended to provide shade, glare control, and weather protection. See *Section 3.8.5.C*.

Buildout, Frontage. Refers to the requirement for a specified percentage of the front or side building face to meet the build-to line or fall within the allowed build-to range. Applicable along streets and public open spaces. See *Section 3.6.1.B.2*.

Build-to line. The conceptual line parallel to a front or side-street property line which must be met by a specified percentage of the building face. See *Section 3.6.1.B.1*.

Chamfered corner. A bevel where two perpendicular building faces meet, resulting in a third face at a 45-degree angle.

Civic Building. A private or public structure of civic importance that is noncommercial in nature. This includes, but is not limited to: schools, places of worship, libraries, museums, city hall, or any other building type that has historically been prominently featured in traditional cities and towns as a center of public life. Civic buildings do not include retail or mixed-use buildings, residential buildings, day care facilities, privately-owned office buildings, or municipal yards. See *Table 3.1.4: Project-Specific Submittal Items & Required Findings*.

Director. The Director of Community Development, or designee.

Downtown Design Review. The process whereby development projects undergo discretionary review by the Planning Commission prior to plan check submittal. See *Section 3.1.4: Design Review*.

Façade. The exterior wall of a building adjacent to a street or public open space.

Façade increment. A complete façade composition that is visually distinguished from an adjacent façade composition of the same building. See *Section 3.7.1.B.2*.

Fenestration. Transparent glass openings in a building face.

Fire Lane. An on-site right of way required by the Fire Marshal for the access of emergency vehicles.

Forecourt. An extension of the public realm into a lot, wherein the space is defined on three sides by building faces (unless located on a corner lot, in which case it might be defined on two sides). See *Section 3.9.3: Forecourt*.

Frontage, Private. The building façade and the area between the building façade and any property line along a street or public open space. See *Section 3.8.1: Private Frontages*.

Frontage, Public. The area between any street-adjacent property line and the first travel lane in the roadway. See *Section 4.3: Public Frontage Design*.

Gallery. A ground-floor colonnade that supports a shed roof or deck that covers the sidewalk, intended to provide shade, glare control, and weather protection. See *Section 3.8.5.D*.

Gratuitous complication (façades). Façade modulations that have no discernible function or meaning. This does not refer to architectural ornamentation, which might include eaves, entablature, bay windows, oriels, window and door surrounds, light fixtures, canopies, balconies, or other historically purposeful aesthetic embellishments.

Ground Floor. The floor of a building located nearest to the level of the existing grade around the building.

Local Symmetry. The balanced distribution of equivalent forms or spaces on either side of an imaginary central vertical line. A locally symmetrical condition occurs only on a portion of the building, and does not refer to the entire building or façade.

Open work fence. A fence in which the solid portions are evenly distributed and constitute no more than 25 percent of the total surface area.

Paseo. A pedestrian path that provides a connection from one right of way or public open space to another. They may be privately maintained or dedicated to the City.

Shopfront. A ground-floor frontage assembly that includes an arrangement of large transparent windows and a conspicuous entrance that leads directly into the unit. Shopfronts are spatially defined on either side by vertical elements called piers.

Passage. An on-site pedestrian path.

Pergola. A wooden structure that is intended to either provide a small area of shade or define an entrance into an on-site outdoor space, such as a front yard or parking lot.

Pier, Shopfront. The vertical elements on either side of a shopfront bay.

Primary building. A building situated towards the adjacent street or public open space that accommodates the primary use of the site. This is distinguished from an outbuilding, which is ancillary in use and form, and sits near the rear of the site.

Public Open Space. Off-street outdoor space that is either owned by the City and intended for public use, or is on a private parcel, immediately adjacent to a right of way, and functions as a public space.

Retail Ready Unit. (Syn. Flex Unit) A Ground floor residential or office unit with a shopfront frontage that is designed to be easily convertible to a retail/service use. This entails high ceilings and an access design that does not preclude future commercial ADA compliance. See *Section 3.8.3*.

Story height. The vertical distance from floor to ceiling. For the topmost story, it is measured from the top of the floor finish to the bottom of the ceiling joists or, where there is not a ceiling, to the bottom of the roof rafters.

Street, Front. The "front" of a property shall be the side of the property that is adjacent to a street. On corner lots, the front shall be the side abutting Miles Avenue, the Retail-Ready Overlay, or any north/south street. A property may have multiple fronts.

Street, Side. (Applicable only to corner lots). The Side street shall be the side of the property that is adjacent to a street or public open space that is not considered the front (see Street, Front).

3.3 Zoning

To generate the physical vision for the Downtown area, this Zoning Map replaces all existing zoning within its boundaries (**Figure 3.3**). The Downtown area is divided into two zones: Downtown Core (DT-C) and Downtown Neighborhood (DT-N). The following sections of this Chapter contain regulations that are specific to each zone. In addition to these Zones, a new Overlay is also established, which provides regulations in addition to those already required by the base Zone. The standards of the Overlay apply only to the side of the lot that the overlay is illustrated on. The intent of each Zone, and of the Retail-Ready Overlay is described below.

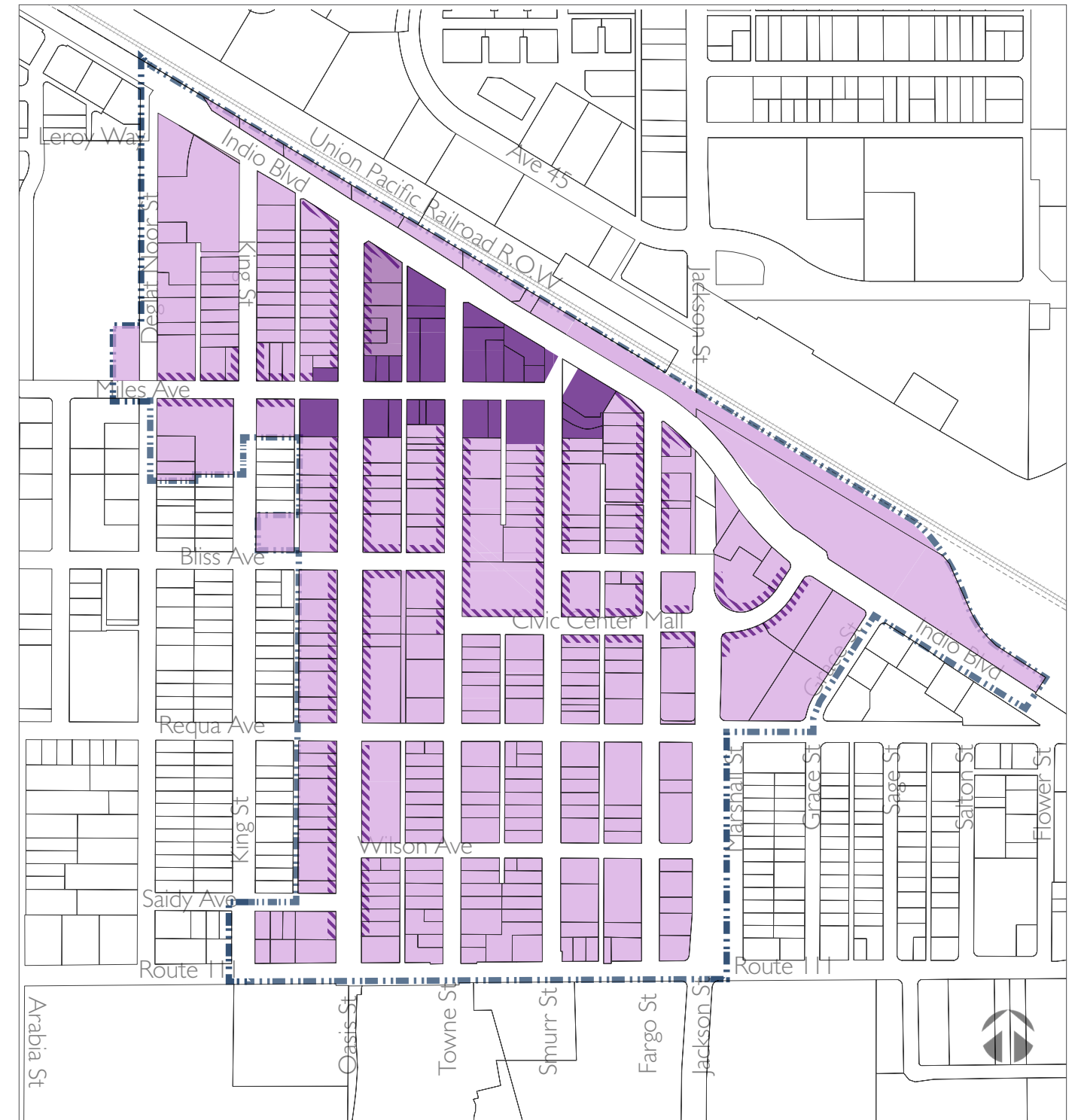
A. Establishment of Zones

- 1. Downtown Core (DT-C).** This zone is characterized by lively, pedestrian-oriented retail, restaurant, service, and similar ground floor uses, with housing and offices on upper floors. Building façades are simple and planar, with ground floor shopfronts set very near the back of sidewalk, with simple arrangements of recessed window openings stacked above the shopfronts, reflecting the traditional 25-foot lotting pattern. Building heights are up to 4 stories, but may reach 6 when community benefits are included. The intent is to concentrate more active ground-floor environment into the historic core along and near Miles Avenue.
- 2. Downtown Neighborhood (DT-N).** This zone is a less intense and more flexible urban environment than the DT-C. Buildings are set back further from the sidewalk, and may have less active uses on the ground floor. Building façades are still simple and planar, and reflect traditional lotting patterns with simple arrangements of recessed window openings stacked above. Building heights are up to 4 stories, but may reach 6 when community benefits are included. The intent of this Zoning District is to generate a comfortable, walkable urban environment that functions and performs well regardless of market conditions.

B. Establishment of Overlay

- 1. Retail Ready (RR-O).** The intent of this overlay is to allow for uninterrupted growth of the downtown commercial environment. Residential and office uses may occupy the ground floor in this overlay, but do not get in the way if/when the market is ready for an expansion of a more active urban environment. To facilitate this flexibility, non-active ground floors are allowed, provided they are built per the standards for "flex" units (see **Section 3.8.3**). These are built to be easily convertible to future retail/service uses, entailing high ceilings and access designs that do not preclude future compliance with the American Disabilities Act for commercial spaces.

Figure 3.3 Zoning Map

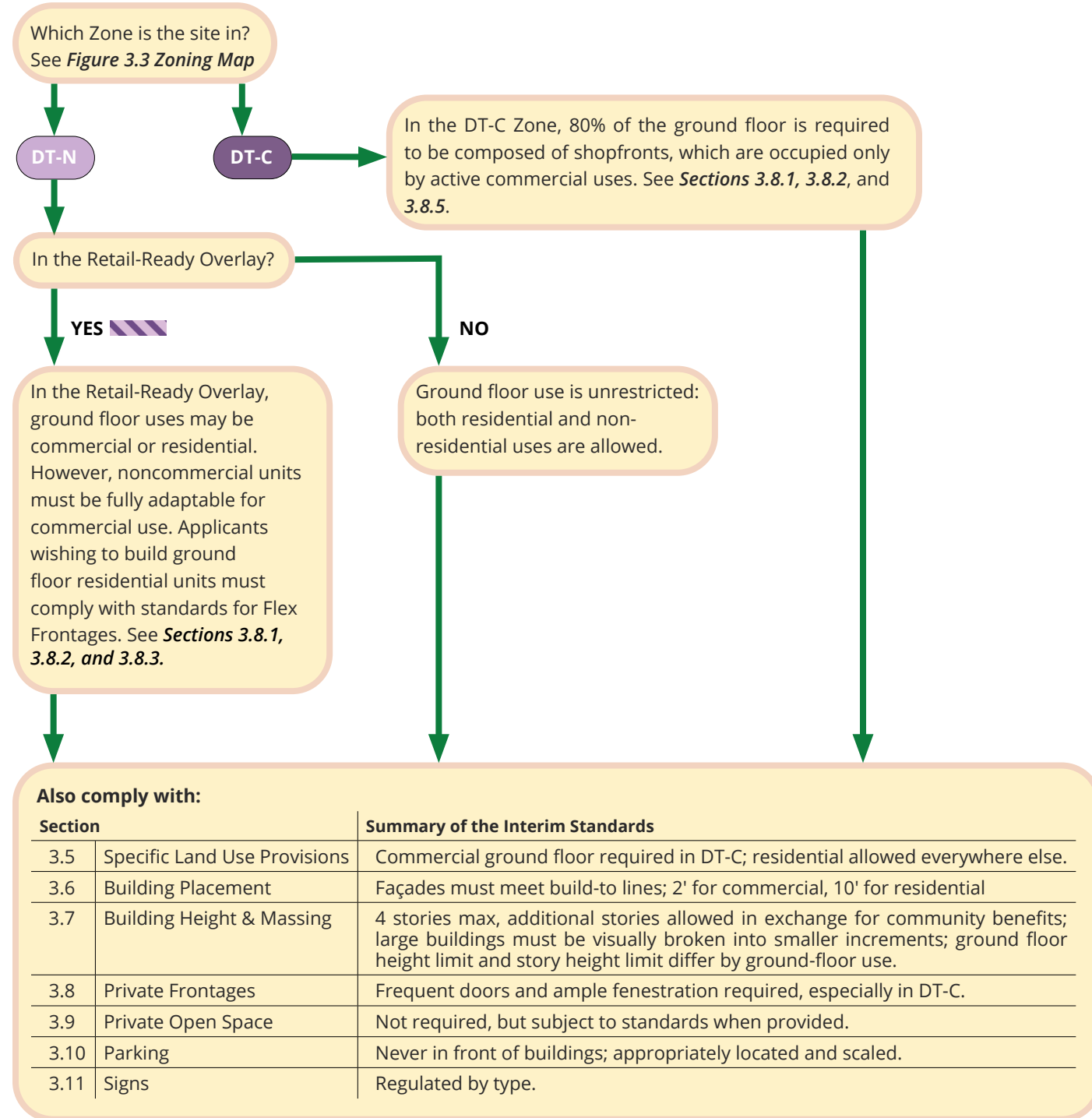


Zone	Overlay
 Downtown Core (DT-C)	 Retail Ready Overlay (RR-O)
 Downtown Neighborhood (DT-N)	

Compliance with Interim Development Standards: Quick Guide

This flowchart is intended to help applicants understand how the Interim Development Standards of this chapter work. It is intended for convenience only. See applicable section(s) for full explanation of each topic.

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Part II Development Standards

Part II provides interim development standards for site development, which are arranged topically. It consists of the following sections:

- 3.4 Development Standards (Introduction)
- 3.5 Specific Land Use Provisions
- 3.6 Building Placement
- 3.7 Building Height & Massing
- 3.8 Private Frontage
- 3.9 On-Site Open Space
- 3.10 Parking & Parking Facilities
- 3.11 Signs

These sections are intended to be used by private developers in the design of their buildings on individual parcels, and for the Planning Department to review construction plans prior to the approval of building permits. The calibration of these interim standards have been informed by **Chapter 2** - The Illustrated Vision, which in turn was informed by the Goals and Policies of the General Plan.

3.4.1 Introduction

The purpose of this section is to demonstrate the logic of each regulatory tool necessary for the successful realization of the physical vision for the Downtown area. Each of the seven topics below play an important role not only in the quality of the resultant development, but in the character of the public realm that each development helps frame. One of the overarching goals for Downtown Indio is that as it is filled out with new buildings and associated public improvements, the environment becomes more active than it is as of the time of the writing of this document—which means more people walking. (Planner and author Jeff Speck explains that for an environment to be "walkable," there are four prerequisites: streets shall be 1) safe, 2) comfortable, 3) interesting, and 4) people have a reason to walk.)¹ The topics below and their standards are specifically tailored to foster such an environment in ways that are contextually appropriate for Downtown Indio.

3.4.2 Intent of Chapters



Specific Land Use Provisions (3.5)

The use regulations of this Chapter include several broad-category uses that are either allowed or restricted by location within the plan area, or within each building. The intent of regulating use by location is to ensure that only the most active ground floor uses occur on the ground floors (so that people have a reason to walk), and concentrate them near the historic core of town. The Retail-Ready overlay ensures that, even though ground floor residential and office uses are allowed, they are easily convertible to commercial uses, and therefore do not "get in the way" when the Downtown commercial environment expands.



Building Placement (3.6)

Because buildings frame the public realm, it is crucial that they are appropriately positioned to form continuous and simple street walls. Without proper definition, streets can lose their sense of comfortable enclosure. These building placement standards, then, prescribe minimum façade buildouts. All buildings should be designed to shape both the public realm and any on-site open space; on-site open space should likewise have as much purpose as the building itself.



Building Height & Massing (3.7)

The height standards address the height of ground floors above the sidewalk, minimum ground story heights, and the maximum number of stories a building may contain. Requirements are calibrated to the ground floor use, and there is built-in flexibility that can accommodate, but does not depend on, growth of the Downtown commercial environment. Massing standards limit façade lengths, with the goal being for large buildings to look like a collection of smaller ones, simulating a more traditional fine-grained urban pattern. This creates more variety and therefore more visual interest.



Private Frontage (3.8)

While a strong, continuous streetwall is necessary to create comfortable enclosure in an urban environment, *how* the enclosing walls relate to the street is just as important. Building façades, their supporting elements, and the space (if any) between the façade and the property line are collectively known as Private Frontage. For the street to *feel* safe, windows are necessary, making people on the street feel seen; for a street to be interesting, there needs to be a variety of active uses and façade treatments, frequent doors, and ample ground-floor fenestration.



On-Site Open Space (3.9)

While it is the intent of this Chapter to regulate buildings so that they properly frame, and therefore enliven the public realm, on-site open spaces are also related to the vision of this Specific Plan. This section provides a vocabulary for the provision of intentional, well-designed open spaces within and around private development. The more urban an environment is, the more important it is that each on-site open space clearly serves a specific purpose. These standards cover dimensions, materials, and other design elements so that these spaces can function well, and contribute to the quality of the overall design of the development.



Parking & Parking Facilities (3.10)

The supply of parking in relation building volume and use can provide necessary convenience for drivers, but if unbalanced and/or contextually insensitive, can also have a negative effect on the urban environment. Standards have therefore been carefully crafted to avoid both an under supply and oversupply of on-site parking. Additionally, placement standards ensure that parking is shaded, and to the extent practical, located either out of view from the right of way, or in small chunks and screened.



Signs (3.11)

On-site signage changes more frequently than buildings do, and can have a significant impact of the character of the environment. The most common sign types are allowed by right, provided they meet the applicable standards. Proposed signs that vary from the standards, or are of a completely different type, may be approved by through Downtown Design Review. See **Section 3.1.4: Downtown Design Review.**

¹ Speck, Jeff. *Walkable City Rules: 101 Steps to Making Better Places*. Island Press, 2018.

3.5.1 Introduction

This section includes land use provisions for all properties within the Plan area. Uses not listed in **Table 3.5: Specific Land Use Provisions** shall be allowed by right unless prohibited elsewhere in the Indio Municipal Code. All uses are subject to all applicable development standards, State law, and any other applicable requirements that are beyond the scope of this Chapter.

Any use that is not specifically prohibited within in this section, but is deemed by the Director to be of such a nature as to be detrimental to neighboring properties by reason of emission of odor, excessive noise, or any other factor that is adverse to their comfort, peace, enjoyment, health, or safety shall also be prohibited.

3.5.2 Use Definitions

Each use listed in **Table 3.5: Specific Land Use Provisions** is defined below. In cases of uncertainty, the Director shall have the authority to make a determination.

Antique Shop. A place of business that sells furnishings, utensils, equipment, objects of art, objects having aesthetic value, ornamental objects, curios and like objects of personal value, all of which by reason of age, antiquity, obsolescence or rarity, are valued principally for decorative, aesthetic or sentimental value or purpose, or as collector’s items, as opposed to the utility value or purpose for which originally manufactured or produced. An antique shall have an age of at least 40 years.

Automobile service. An establishment for which the primary purpose is the on-site repair, cleaning, detailing, dismantling, or similar work on cars.

Bar/nightclub. An establishment for which more than 50% of total gross revenues per year are from alcohol sales for on-site consumption.

Church. An facility for which regular religious services are the primary function.

Drive-through facility. An establishment where customers are regularly attended while remaining seated within an automobile.

Dwelling unit. Any building or portion thereof that is used as an independent living facility for one or more persons.

Liquor Store. A retail establishment which has 25% or more of the shelving devoted to the public display and sale of alcoholic beverages for off-site consumption.

Heavy industrial/manufacturing. An establishment that includes labor-intensive manufacturing, assembly, fabrication, or repair processes that produces odors, noise, vibration, hazardous waste materials, or particulates that may negatively affect other uses on the same site or neighboring properties.

Office. A building or portion thereof used as a place for commercial, professional, or bureaucratic work that does not depend on regular on-site visits from customers.

Thrift Shop. Any individual personal partnership, firm or corporation whose business includes buying, selling, trading, taking in pawn, accepting for sale on consignment, or accepting for auctioning, secondhand tangible property.

Storage/warehouse. A facility for which the primary purpose is storage, wholesaling, and/or distribution.

Vehicle fueling facility. An automobile-refueling facility.

Table 3.5 Specific Land Use Provisions

Uses	Zones	
	DT-C	DT-N ¹
Antique Shop	Not permitted anywhere in zone	Administrative CUP required
Automobile service	Not permitted anywhere in zone	Not permitted anywhere in zone
Bar/nightclub	Administrative CUP required	Administrative CUP required
Church	Not permitted anywhere in zone	CUP Required
Drive-through facility	Not permitted anywhere in zone	Not permitted anywhere in zone
Dwelling Unit	Not permitted on ground floor	Permitted anywhere in zone
Liquor Store	Administrative CUP required	Administrative CUP required
Heavy industrial/manufacturing	Not permitted anywhere in zone	Not permitted anywhere in zone
Office	Permitted on upper floors; Administrative CUP required for ground floor	Permitted anywhere in zone
Storage/warehouse	Not permitted anywhere in zone	Not permitted anywhere in zone
Thrift Shop	Not permitted anywhere in zone	Administrative CUP required
Vehicle fueling facility	Not permitted anywhere in zone	Not permitted anywhere in zone

Key

CUP Conditional Use Permit. See IMC § 159.750-765 for regulations regarding CUPs, and IMC § 159.770-777 for regulations regarding Administrative CUPs.

Notes

¹ All ground-floor residential and office units in the Retail-Ready Overlay (RR-O) shall be designed as retail-ready units. (See **Section 3.8.3**).

3.6.1 Building Placement

A. Standards

1. All new lots shall front a street or public open space.
2. Rear access shall be preserved or provided for each new development. Reconfiguration of the original alleys may only be allowed through Downtown Design Review. Surface parking lots and drive aisles may satisfy this requirement, provided that they connect through the block. See **Section 3.1.4: Downtown Design Review**.
3. Specific to nonresidential ground-floor uses: any space provided between the building façade and the street-abutting property line shall be to effectively widen the sidewalk; no in-ground landscaping or other permanent obstructions are permitted unless specified in Private Frontages (**Section 3.8**).
4. Building Orientation & Access.
 - a. All buildings adjacent to a street or public open space shall orient toward, and take primary access from the street or public open space.
 - b. Buildings and ground floor units (residential or commercial) shall not orient toward a parking lot, access drive, or fire lane.

B. Measurements

1. "Build-to Line" is the conceptual line parallel to a front or side-street property line which must be met by a specified percentage of the building face(s).
 - a. Numeric values represent the distances from adjacent property line.
2. "Frontage Buildout" is the portion of the façade that shall meet the build-to line(s). This façade shall be supported by occupiable space behind.
3. The "front" of a property shall be the side adjacent to a street or public open space. For corner lots, the front(s) shall be that which is applicable below:
 - a. the side abutting Miles Avenue;
 - b. the side abutting the Retail Ready Overlay;
 - c. the side abutting a north/south street.

A property may have multiple fronts.

C. Exceptions

1. The following elements may recess or break the façade, and constitute allowed exceptions to the frontage buildout requirement.
 - a. Forecourts, subject to the standards in **Sections 3.8.4.D and 3.8.5.F**.
 - b. Entrances to passages, subject to the standards in **Section 3.9.6**.
 - c. Entrances to paseos, subject to the standards in **Section 4.4.B.3**.
 - d. Ground-floor recessions, provided that it is part of a Frontage Variation. See **Section 3.8.2**.
 - e. Upper floor recessions for usable on-site open space. See **Section 3.9**.
 - f. Upper floor massing breaks, in order to achieve façade differentiation per **Section 3.7.1.A.4**.
 - g. Façade articulation, such as window or wall recesses or projections, provided that the variation does not exceed 18 inches.
 - h. The width of exterior staircases and associated landing areas.
 - i. Chamfered corners, provided they include a primary entrance or stairway.
 - j. DT-N Zone Only: the frontage of a side yard, subject to the standards in **Section 3.9.4**.
 - k. DT-N Zone Only: the frontage of a surface parking lot and associated access, subject to the standards in **Section 3.10**.
2. Encroachments past the build-to lines are allowed in **Section 3.8**.

Figure 3.6 Building Placement

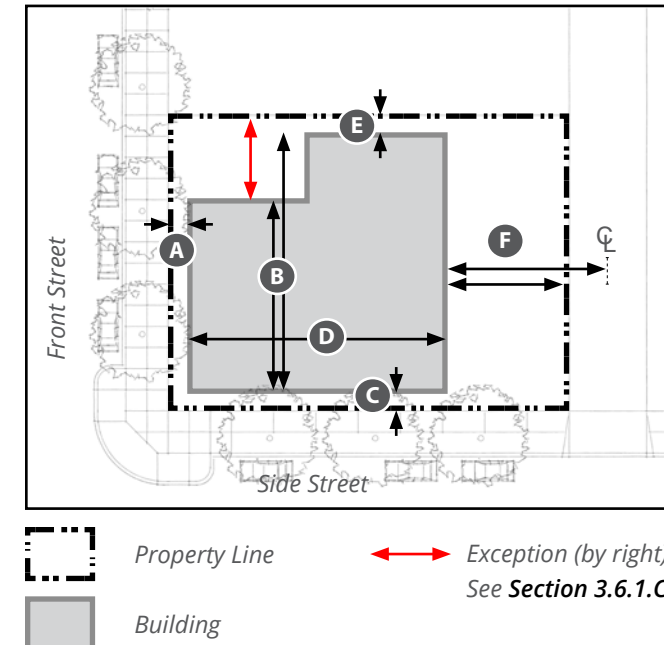


Table 3.6 Building Placement

	DT-Core	DT-Neighborhood ¹	RR-O ²
Build-to Requirements			
A Front street build-to line	2'	10'	2' or 10'
B Front street frontage buildout (minimum percentage of lot width)	100%		
C Side street build-to line (corner lots only)	2'	2' or 10'	
D Side frontage buildout	Shall meet build-to line for the first 40' behind front façade.		
Setbacks (min.)			
E Side yard	None	10' or 10% of lot width ³	
F Rear with alley access	15' from alley centerline		
Rear without alley	min. 5'		

Notes

- 1 The build-to line is 15' from all property lines parallel and adjacent to Highway 111.
- 2 The build-to line is 10' from property line wherever a residential unit occupies the ground floor adjacent to a street or public open space; this does not apply to upper floors.
- 3 Whichever is less.

3.7.1 Building Height & Massing

A. Standards

1. One-story buildings are permitted, but shall reach an exterior height of a minimum of 20 feet along the entire frontage buildout as measured from finished grade.
2. Within the DT-Core Zone, ground floors shall be a minimum of 12 feet in height.
3. The ground floor of units fronting on the Retail Ready Overlay shall be 12 feet tall and designed so that future compliance with the Americans with Disabilities Act of 1990 (ADA) for commercial use is not precluded. See "Flexible Ground Floors" in **Section 3.8.3**.
4. Façade increment. Buildings greater than 100 feet in length are required to visually differentiate the façade into distinct compositions, each being 100 feet or less. (This is not applicable to parking structures. Standards for parking structures are in **Section 3.10**.) Two or more of the following methods shall be used to meet this requirement:
 - a. Massing breaks, formed by recessions at least 15' wide and 10' deep (may occur above ground floor);
 - b. Different materials;
 - c. Different roof heights;
 - d. Windows: different sizes, shapes, muntin patterns;
 - e. The differentiation and/or misalignment of horizontal architectural features, like cornices and string courses;
 - f. Any other differentiation that meets the intent of the standard (see **Section 3.4**) to the satisfaction of the Director.

B. Measurements

1. Story height is measured from floor to ceiling. For the topmost story, it is measured from the top of the floor finish to the bottom of the ceiling joists or, where there is not a ceiling, to the bottom of the roof rafters.
2. Nonresidential and residential ground floor. This refers to the ground floor space adjacent to the street.

C. Exceptions

1. Buildings may provide one or two complete floors above the height limit in exchange for specified community benefits. See **Section 3.1.6: Community Benefits Height Bonus**.
2. The following architectural elements may exceed the height limits by right.
 - a. Penthouses or roof structures for the housing of elevators, stairways, tanks, ventilating fans or similar equipment required to operate and maintain the building, fire or parapet walls, skylights, bell towers, church steeples, chimneys, or similar structures.

Figure 3.7-I Height

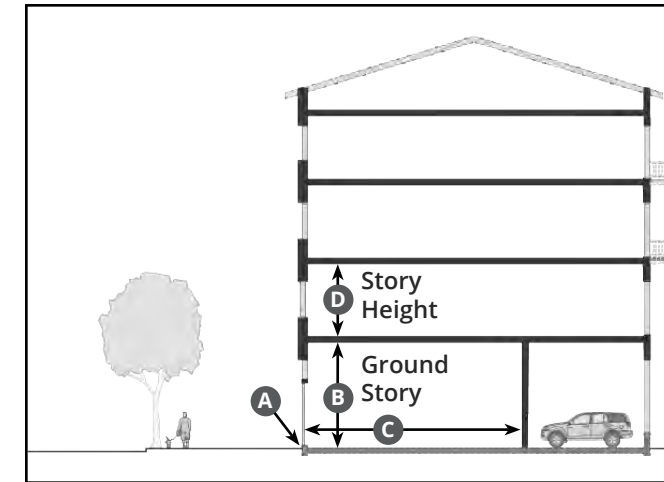


Figure 3.7-II Massing

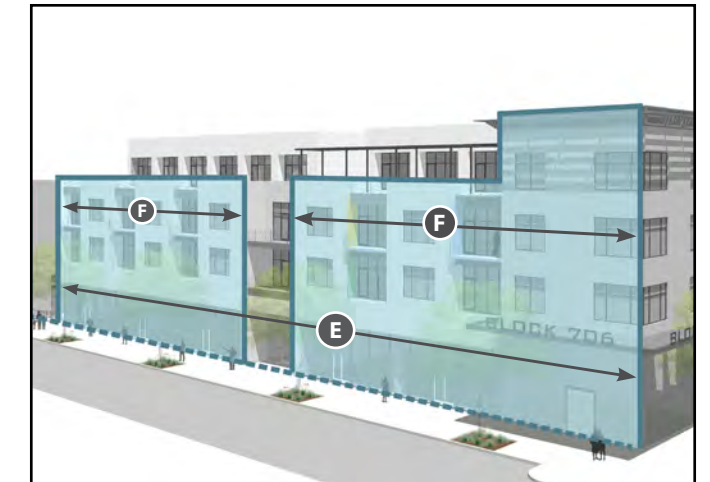


Table 3.7 Building Height & Massing

Stories	Nonresidential Ground Floor	Residential Ground Floor
Stories	4 max	
A Ground floor above grade at building setback line	3.5' max	5' max
B Ground story height	12' min	9' min ¹
C Ground story depth	30' min	
D Upper story height	8' min	
E Building length (max)	400'	
F Façade increment (max)	100'	

Notes

¹ Minimum 12' in RR-O

3.8.1 Private Frontages & Facades

A. Applicability. Private frontage and façade standards pertain to the façade of each building that fronts or sides on a street or public open space, and the space between the façade and frontage property line. The following standards apply to all private frontages and facades, with the exception of parking structures. Standards for parking structures are in **Section 3.10**.

B. Access. All buildings shall take primary access from the sidewalk. External stairways may satisfy the requirements below; stairways and entrances on chamfered corners may also satisfy the requirements below, but shall not be counted for both façades. Fire exit, service, utility, and similar doors do not satisfy these requirements.

1. At least two entrances shall be provided for each façade increment along Miles Avenue, the Retail-Ready Overlay, and all north/south streets. Entrances shall be no more than 25 feet on center. (See **Section 3.7.1.B.2**)
2. On east-west streets, at least one entrance is required for façades greater than 75 feet in length.

C. Materials. All potentially reflective building materials and surfaces shall be painted or otherwise treated to minimize reflectivity. Façade materials shall be durable, and not require frequent repainting or replacement. Natural ground floor materials such as stone, brick, glass, smooth stucco, concrete and ceramic tile are recommended where pedestrians may come into direct contact with wall materials.

D. Fenestration. All façades fronting or siding on a street or public open space shall be articulated by frequent openings.

1. **Design.**
 - a. Except for shopfront and bay windows, glass shall be recessed from its framing by a minimum of 3 inches.
2. **Frequency.** The horizontal distance between any neighboring openings within the same façade increment shall not exceed 10 feet, as measured from the edge of the glass. See **Section 3.7.1.B.2**.
3. **Glass transparency.** All ground-floor windows shall have an external reflectance of less than 15%, and ground floor windows shall additionally have a transparency of 80% or greater.
4. **Façade coverage.** The following ranges prescribe the required percentage of glass area within any 50-foot increment of façade. "Ground floor" is the entire façade area between the interior floor and ceiling; "upper floors" refers to the entire façade area from the interior floor of the second floor to the ceiling of the top floor.
 - a. Ground floors in the Downtown Core Zoning District (DT-C), Retail-Ready Overlay (RR-O), and any nonresidential space elsewhere: minimum 60%. Sills of windows satisfying this requirement may be no higher than 3 feet from adjacent interior finished grade.
 - b. Ground-floor residential not in the Retail-Ready overlay: minimum 15%. Sills of windows satisfying this requirement may be no higher than 3 feet from adjacent interior finished grade.

- c. Upper floors (any use): minimum 20%, which may include windows within balconies.
- d. Upper floor windows shall be generally stacked above and/or organized in clear relation to the façade increments of ground floor openings identified in this section, and to the massing increments identified in **Section 3.7**.

E. Fences and walls.

1. **General.** Unless otherwise permitted within this Chapter, fences and site walls shall not exceed 3.5 feet in height from lowest adjacent grade between any building and street or public open space. Fences and site walls shall not exceed 6 feet from adjacent finished grade anywhere else on the property.
2. **Materials.** Chain link is prohibited within 15' of any street or public open space. Barbed wire shall not be allowed anywhere.
3. **Specific to surface parking lots.** (see also **Section 3.6: Building Placement** and **Section 3.10: Parking & Parking Facilities**).
 - a. Any portion of a surface parking lot adjacent to a street shall be screened by a wall not exceeding 4 feet in height, and behind a landscaped setback of at least 2 feet.
 - b. Security fencing up to 8 feet in height is permitted, but shall be open work (see **Section 3.2: Glossary**).

- c. Walls shall be interrupted only for vehicular and pedestrian access & visibility.
- d. Architectural features, such as pergolas may frame pedestrian or vehicular entrances, and shall not exceed a height of 10 feet.

F. Grading. The average slope between the façade of a building and the right of way shall only be between 0.5% and 5%.

G. Clutter. All utilities, above-ground equipment, and trash containers shall be located in the rear of the lot. Rooftop equipment screening shall be integrated into the building architecture and shall not be visible from the street level or adjacent buildings.

H. Frontage elements. The following sections specify an array of frontage elements that are appropriate for the Downtown area. In addition to the standards of this section, the standards specific to each frontage variation (see **Sections 3.8.2 - 3.8.5**) apply wherever such variation is proposed.

1. **DT-Core.** 80% of the façade satisfying the buildout requirement shall be shopfront assemblies per **Section 3.8.5.A**.
2. **Retail Ready Overlay.** 80% of the façade satisfying the buildout requirement shall be shopfront assemblies per **Section 3.8.5.A**; all ground floors adjacent to a street or public open space in this overlay shall be either designed for immediate commercial occupancy, or for future conversion to commercial occupancy. See **Sections 3.8.2 - 3.8.5**.



An example of the form (not style) intended for the Retail-Ready Overlay. Where the market is not ready for retail, ground floors may be designed for residential or office use in the meantime.



To ensure that an environment is interesting to walk through, commercial ground floors shall have frequent doors and clear and ample fenestration.



A forecourt is a good exception to the buildout requirement. Low walls can provide the appropriate amount of privacy while allowing visual interest from either side of the property line to the other side.



Recessed shopfronts can provide opportunities for active uses like outdoor dining, without interrupting the streetwall.

3.8 Private Frontages & Facades

3.8.2 Private Frontage Variations - Introduction

A. Intent. These variations represent the array of ways in which building façades relate to, and engage with the public realm in traditional urban environments. The intent is that frontage elements are appropriately scaled, and contribute to the intended character of the urban environment.

B. Applicability. Depending on the intended ground floor use and the associated regulations, some of the frontage elements covered in this section will work, while others will not. Wherever each frontage element occurs, the standards specific to that element shall apply.

Figure 3.8.2 Private Frontage



3.8.3 Retail-Ready Overlay: Flexible Ground Floors

In the Retail-Ready Overlay (see Fig. 3.3), residential and office uses are only permitted on the ground floor if built flexibly. See Figures 3.8.3.A and 3.8.3.B for examples.

For such "flex" units, the following standards shall apply:

- The front setback is exactly 10 feet for any residential unit(s).
- Future conversion to ADA-compliant retail/service use shall not be precluded by the interim design.
- Ground floor story height is a minimum of 12' from finished floor to ceiling.
- All standards for shopfront frontage are met (see Section 3.8.5.A).

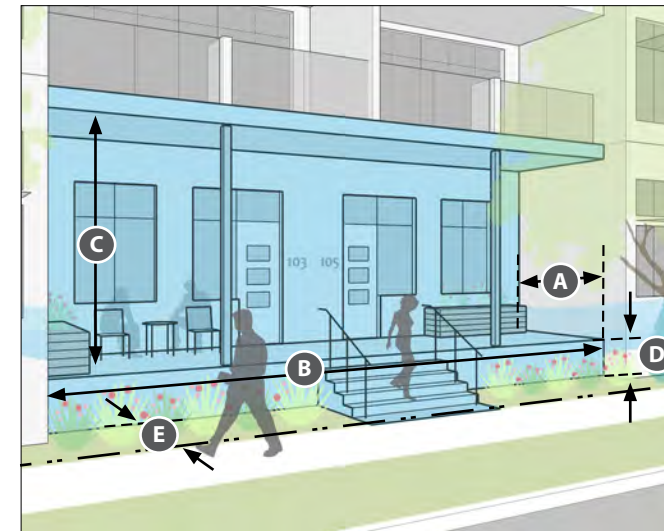
Figure 3.8.3.A Dooryards / Terraces Figure 3.8.3.B Arcades



3.8.4 Residential Private Frontages

These variations are allowed for residential ground floors. They provide privacy and transitional space between the public and private realms.

A. Porch



Porches are roofed, unenclosed rooms attached to the exterior of a building that provide building access and semi-private outdoor social space that help activate the public realm.

Standards	Min	Max
A Porch depth (excluding stairs) ¹	8'	10'
B Porch width	10'	-
C Porch height ²	8'	12'
D Floor height ³	0'	3.5'
E Between porch and front PL	5'	-

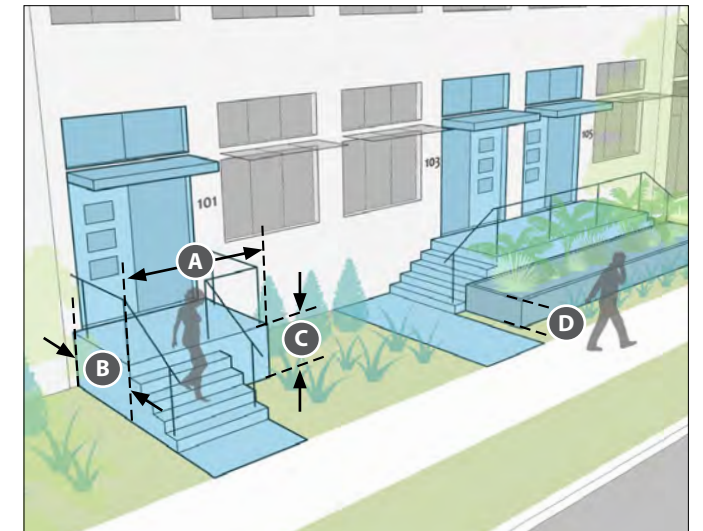
Additional Requirements

- A porch may be wrapped by railing and/or walls: solid walls shall not exceed 3.5 feet from adjacent exterior grade; open work fences (as defined in Section 3.2: Glossary) shall not exceed 3.5' from deck height
- A porch may wrap around building corners.

Notes

- 1 Between building façade and end of porch deck.
- 2 From porch floor to top of porch columns.
- 3 Measured from grade at right of way.

B. Stoop



A stair and landing leading directly from the right of way to an elevated building entrance.

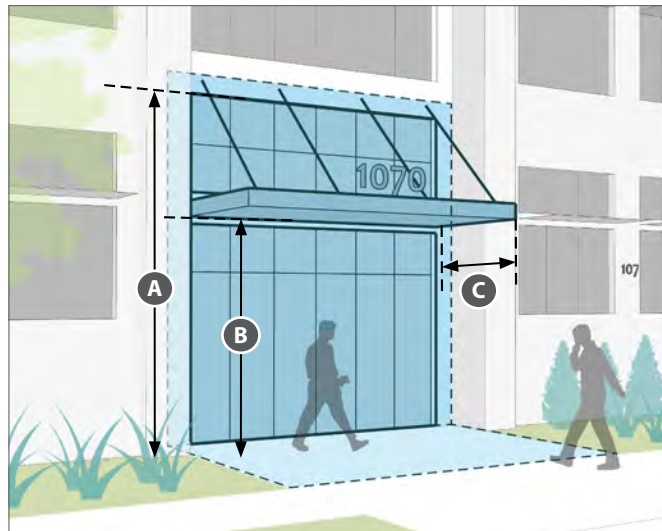
Standards	Min	Max
A Stoop width	4'	8'
B Stoop depth (not including stairs)	4'	8'
C Stoop floor height	1.5'	3.5'
D Planter/fence height	-	3'
Entry Recession depth	0.5'	6'
Between edge of landing and front property line	1'	-

Additional Requirements

- Stoop enclosure shall only be open work (as defined in Section 3.2: Glossary) and shall not exceed 3.5' in height from supporting surface; solid walls and gates are prohibited.
- The exterior stairway may be perpendicular or parallel to the adjacent sidewalk. When parallel to the sidewalk, landscape of at least 1 foot shall be provided between the side of stair/stoop and the sidewalk.

3.8.4 Residential Private Frontages (Continued)

C. Common Entry



A common entrance is a doorway to a lobby that provides access to upper-floor units.

Standards	Min	Max
A Height to top of transom	12'	20'
B Height to bottom of canopy/awning	8'	-
C Awning/Canopy Depth	4'	-
Entryway recess	1'	20'
Additional Requirements		
• Common entries should be locally symmetrical (see <i>Local Symmetry</i> in Section 3.2: Glossary).		

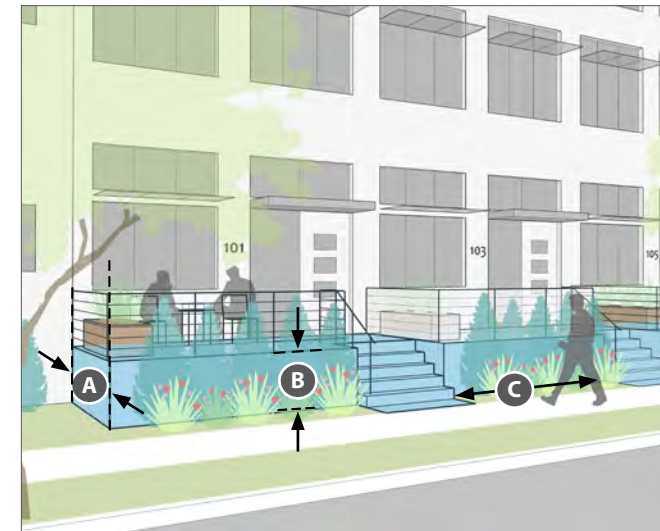
D. Forecourt



A forecourt is an extension of the public realm into the private property. The building fronts onto and receives access from this space.

Standards	Min	Max
A Width of court area	15'	-
B Depth of court area	20'	-
Additional Requirements		
• A forecourt shall be defined on three sides by buildings, unless located on a corner lot, in which case it shall be defined on two sides. One of these sides may be a neighboring building.		
• Walls that enclose or are located within forecourts shall not exceed 3.5 feet in height from the lowest adjacent grade. Entryway pergolas are allowed exceptions to this height limit.		
• Frontage elements and exterior stairways shall not encroach into the required 15' x 20' area.		

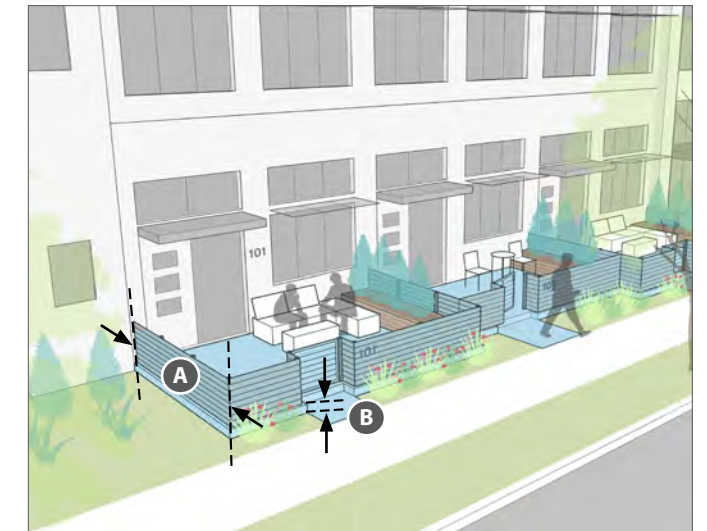
E. Terrace



These are areas adjacent to the ground floor of a façade that is enclosed by a low wall or fence. They may be at grade or raised, depending on the height of the ground floor of the building. See **Figure 3.8.3.A** for an example of how dooryards may be used to create flexible (retail-ready) ground floor space.

Standards	Min	Max
A Depth, clear	8'	-
B Finish level above sidewalk	-	3.5'
C Distance between stairways	-	50'
Length of terrace	-	150'
Between edge of landing and front property line	1'	-
Additional Requirements		
• Terraces not providing direct access to the public open space or right of way are allowed.		
• Railing shall be open work (as defined in Section 3.2: Glossary) not exceed 3.5' in height from supporting surface.		

F. Dooryard

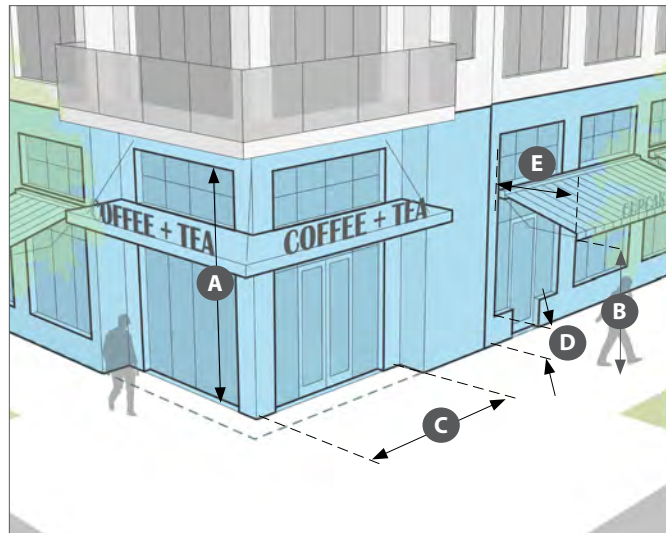


Standards	Min	Max
A Depth, clear	8'	-
B Finish level above sidewalk	-	At grade
Length of dooryard	-	1 per unit
Between edge of landing and front property line	1'	-
Additional Requirements		
• Enclosures shall not exceed 3.5 feet in height from the lowest adjacent grade.		
• Enclosures may be removed if the space is converted to nonresidential use.		

3.8.5 Commercial Private Frontages

The following private frontage variations are appropriate for commercial ground floors. Retail-ready units shall provide shopfront frontages, and be designed to not preclude future conversion to retail/service use in compliance with the American Disability Act.

A. Shopfront



The basic form of a shopfront is a large opening in the façade with large clear windows, doors with glass, transom windows, and a solid shopfront base. "Fenestration" refers to the area of glass.

Standards	Min	Max
A Height to top of transom	12'	20'
B Height to bottom of canopy/awning	8'	-
C Width of each shopfront bay	10'	25'
D Height of shopfront base	1'	3'
E Awning/Canopy Depth	4'	-
Fenestration per bay (from pier centerlines, floor to ceiling)	70%	-

Additional Requirements

- Façades shall be divided into bays, each of which contains its own coherent assembly between piers. An entrance is not required in each bay.
- Awnings and Canopies (not required) may encroach upon the public right-of-way up to 2' from the curb.
- Entrances may be recessed.

B. Recessed Shopfront



A recessed shopfront allows for covered semi-public space for dining or other activities.

Standards	Min	Max
A Depth of recessed area	8'	25'
B Height to bottom of opening	8'	-
C Width of recessed area	6'	25'

Additional Requirements

- All shopfront standards (*Section 3.8.5.A*) apply.

C. Arcade



An arcade is a façade with a ground floor colonnade that supports the upper stories of the building. Arcades provide shade, glare control, and weather protection. See *Figure 3.8.3.B* for an example of how dooryards may be used to create flexible (retail-ready) ground floor space.

Standards	Min	Max
Height (sidewalk to ceiling)	12'	-
A Depth (façade to interior column)	10'	16'
B Setback from edge of curb	2'	3'

Additional Requirements

- Column height shall be between four and five times the column width.
- Column spacing shall correspond to shopfront piers to the extent possible.
- Arcades shall only be utilized in combination with shopfront frontage.

D. Gallery



A gallery is a ground-floor colonnade that supports a shed roof or a deck that covers the sidewalk. Galleries provide shade, glare control and weather protection.

Standards	Min	Max
Height (sidewalk to ceiling)	12'	-
A Depth (façade to interior column)	10'	16'
B Setback from edge of curb	2'	3'

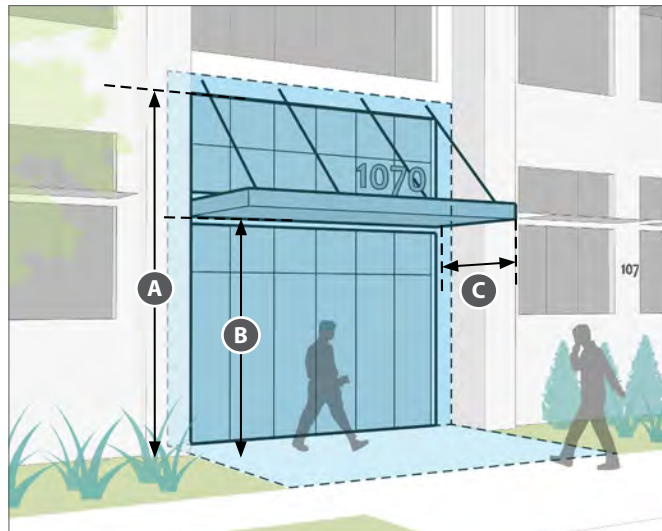
Additional Requirements

- Column height shall be between four and five times the column width.
- Column spacing shall correspond to shopfront piers to the extent possible.
- Galleries shall only be utilized in combination with shopfront frontage.

3.8.5 Commercial Private Frontages (Continued)

These variations may be appropriate for the ground floors of buildings regardless of use.

E. Common Entry



A common entrance is a doorway to a lobby that provides access to upper-floor units or other large nonresidential space.

Standards	Min	Max
A Height to top of transom	12'	20'
B Height to bottom of canopy/awning	8'	-
C Awning/Canopy Depth	4'	-
Entryway recess	1'	20'
Additional Requirements		
• Common entries should be locally symmetrical (see <i>Local Symmetry</i> in Section 3.2: Glossary).		

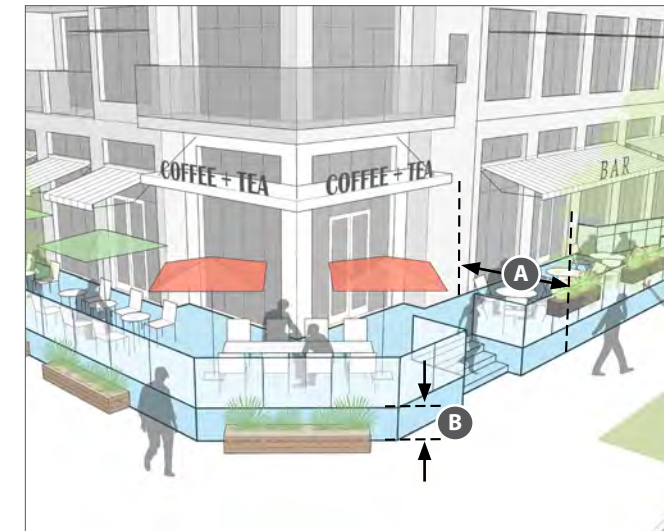
F. Forecourt



A forecourt is an extension of the public realm into the private property. The building fronts onto and receives access from this space.

Standards	Min	Max
A Width of court area	15'	-
B Depth of court area	20'	-
Additional Requirements		
<ul style="list-style-type: none"> • A forecourt shall be defined on three sides by buildings, unless located on a corner lot, in which case it shall be defined on two sides. One of these sides may be a neighboring building. • Walls that enclose or are located within forecourts shall not exceed 3.5 feet in height from the lowest adjacent grade. Entryway pergolas are exceptions to this height limit. • Buildings with forecourts and ground-floor commercial uses may be enclosed during non-business hours by an open work fence (as defined in Section 3.2: Glossary) not exceeding 8 feet in height. • Frontage elements and exterior stairways shall not encroach into the required 15' x 20' area. 		

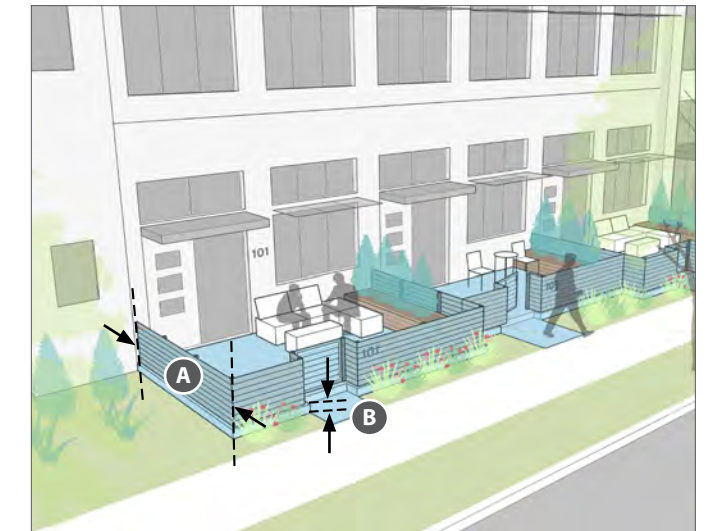
G. Terrace



These are areas adjacent to the ground floor of a façade that is enclosed by a low wall or fence. They may be at grade or raised, depending on the height of the ground floor of the building. Commercial terraces must comply with American Disabilities Act requirements, and may not be feasible in many contexts.

Standards	Min	Max
A Depth, clear	8'	-
B Finish level above sidewalk	-	3.5'
Distance between stairways	-	50'
Length of terrace	-	150'
Between edge of landing and front property line	1'	-
Additional Requirements		
<ul style="list-style-type: none"> • Terraces not providing direct access to the public open space or right of way are allowed. • Railing shall be open work (as defined in Section 3.2: Glossary) not exceed 3.5' in height from supporting surface. 		

H. Dooryard



These are areas adjacent to the ground floor of a façade that is enclosed by a low wall or fence. They may be at grade or raised, depending on the height of the ground floor of the building. Commercial terraces must comply with American Disabilities Act requirements, and may not be feasible in many contexts.

Standards	Min	Max
A Depth, clear	8'	-
B Finish level above sidewalk	-	3.5'
Length of dooryard	-	1 per unit
Between edge of landing and front property line	1'	-
Additional Requirements		
• Enclosures shall not exceed 3.5 feet in height from the lowest adjacent grade.		

3.9.1 On-Site Open Space

A. Description. On-site open space is private or shared outdoor space that exists for the enjoyment and use of building tenants, and sometimes the public. It can also provide comfortable paths through which pedestrian access is provided from the street to any buildings (or portions of buildings) that lack direct street frontage. This Section identifies a series of open space types and design characteristics of each type.

B. General regulations. The following regulations and recommendations shall apply to all on-site open space.

1. Landscape. All landscape and irrigation designs shall comply with the following:

- a. Recommended species.** Trees and Plant material shall either be native to the Sonoran Desert, or adaptive and from regions with similar arid climates (such as the Mojave Desert).
- b. Invasive species.** Invasive plant species shall be prohibited.
- c. Desert-adapted plants.** All plants shall be water-conserving, drought-tolerant plants.
- d. Green screens.** Landscape shall be used to soften walls and fences and provide green screens, where appropriate, between residential and nonresidential buildings.
- e. Irrigation.** Permanent and automatic irrigation or drip irrigation systems shall be utilized for all landscaped areas.
- f. Climate mitigation.** Trees, shrubs, hedges, and deciduous vines should be used to minimize solar heat gain during the summer and maximize heat gain during the winter.
- g. Crime Prevention through Environmental Design (CPTED).** All landscape plans shall be reviewed by a CPTED-qualified member of the Indio Police Department.

2. Outdoor lighting.

- a. Lighting shall be shielded by permanent attachments to fixtures so that light sources are not visible from adjacent properties or the public right of way.



A drought-tolerant backyard.



Drought-tolerant landscaping.



Native, desert-adapted plants should be used.

b. Specific to surface parking lots, private frontages, and any other on-site open space that abuts a public right of way.

- i. Lighting shall be shielded by permanent attachments to fixtures so that light sources are not visible from adjacent properties or the public right of way.
- ii. No freestanding fixture shall exceed a height of 15 feet as measured from finished adjacent grade.
- iii. Light sources shall have a maximum output of no greater than 3,000 lumens.
- iv. Required illuminance (in footcandles)
 - Max: 4.0
 - Min: 0.2
 - Max average: 0.8
 - Uniformity ratio (max/min): 20/1
 - Uniformity ratio (average/min): 4/1

3. Fences and garden walls. Fences and garden walls within and enclosing on-site open spaces shall comply with **Section 3.8.1.E**.

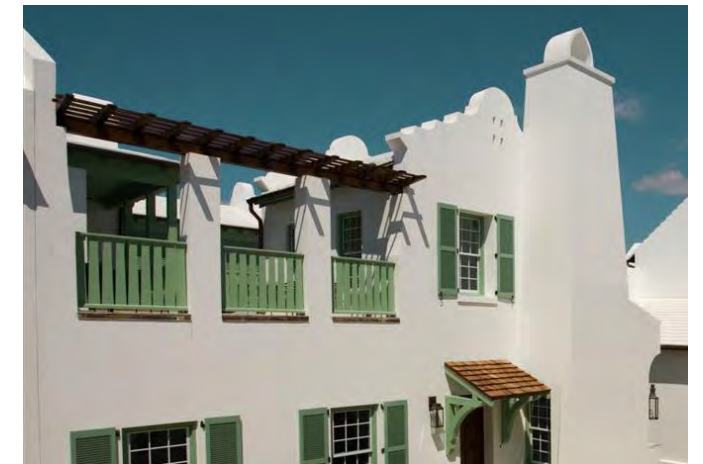
4. Sustainable stormwater management. On all new building sites, ground water recharging shall be facilitated and stormwater runoff shall be limited. Possible strategies include rain gardens, vegetated bioswales, and pervious paving materials. Decorative water features shall use re-circulating water and recycled water where possible.

C. On-site open space types. These types will be identified on the following pages.

- Court
- Forecourt
- Side Yard
- Rear Yard
- Passage
- Roof Deck/Terrace



On-site light sources should be shielded from the public ROW.



Roof terrace is consistent with the character of the building.



A parking lot paved with gravel allows water to infiltrate, reducing runoff.

3.9.2 Court

A. Description. A court is an open space surrounded by one or more buildings, for use by residents or tenants, or for activation as a public plaza or outdoor dining area. It can provide visitor access from the street to dwellings, retail, office spaces, or buildings within the lot that lack direct access from the street.

B. Specifications. A court's perimeter shall be coherent and well-defined by walls on at least three sides. Courts shall include a minimum of one shared amenity, such as a seating area, fountain, BBQ island, or outdoor fireplace.



A court may provide access to private residences.



A court can function as a shared dining area for residents or restaurants.

3.9.3 Forecourt

A. Description. A forecourt is a court that abuts the public sidewalk. Where forecourts give access to retail and other public uses, they function as an extension of the public realm. Where forecourts grant access to residential or other private uses, they function as transitional spaces between the public and private realms.

B. Specifications. See *Section 3.8.4.D and 3.8.5.F.*

C. Build-Out Requirements. A forecourt is a permitted exception to the build-out requirement for a lot. See *Section 3.6.1.C.*



Forecourts help transition from the public to private realm.



When used as an outdoor dining area, a forecourt should read as an extension of the public realm.

3.9.4 Side Yard

A. Description. A side yard is an open space along one side of a building. They are semi-private spaces through which visitor access is provided to one or more buildings or dwellings, or they are private spaces for the exclusive use of the residents of one or more dwellings.

B. Specifications. Side yards shall be defined by buildings on a minimum of two sides.

C. Standards. Side yards shall not exceed twenty-five feet in width where adjacent to streets or public open spaces.

D. Build-Out Requirements. A side yard is a permitted exception to the build-out requirement for a lot in the DT-N Zone. See *Section 3.6.1.C.*



Single- and multi-family residences separated by side yards.

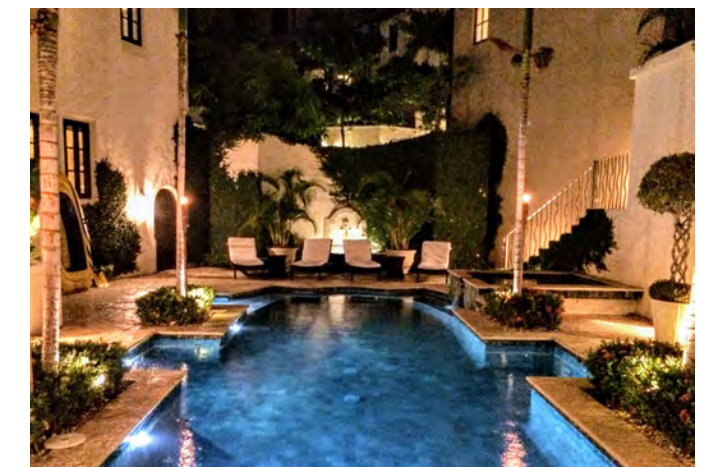


The side yard of this multi-family building provides access to adjoining units.

3.9.5 Rear Yard

A. Description. A rear yard is a private, landscaped open space located behind a building. It is for the use of the residents or users of one or more dwellings.

B. Specifications. For buildings with two or more units, a rear yard may be divided into separated private yards, provided each private yard is directly adjacent to the dwelling unit.



Rear yards can contain amenities for multi-family buildings.



This rear yard provides on-site open space for the ground floor unit, while a terrace provides open space for the unit above.

3.9.6 Passage

A. Description. Passages provide a pedestrian connection between or through buildings, from the street to a court, recessed entrance, or rear parking lot. Passages may be covered or uncovered. They may be gated or completely open to the street.

B. Specifications. They shall be provided between buildings or along side yards.

C. Standards. Passages shall be a minimum of six feet in width.

D. Build-Out Requirements. A passage is a permitted exception to the build-out requirement for a lot. See *Section 3.6.1.C*.



A passage may provide access to residential units.

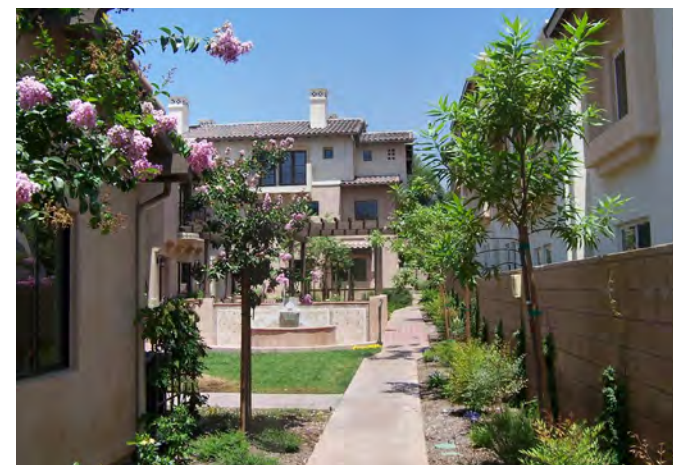
3.9.7 Roof Deck/Terrace

A. Description. A roof deck or terrace is an outdoor gathering space that may be assigned to individual units or shared by all residents or tenants of a building.

B. Recommendations. Roof decks shall include a minimum of one amenity and design element, such as a trellis, seating area, fountain, landscaping, or outdoor fireplace to encourage their use as an outdoor gathering place.



A roof deck/terrace used as a dining area for restaurants.



A passage may connect between courtyards and the street.



Roof decks/terraces may provide shared lounging and dining areas for residents.

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3.10.1 Applicability

A. The standards of this section are intended to supplement the standards in IMC § 159.563. Wherever standards conflict, the standards of this Chapter shall prevail.

3.10.2 On-Site Parking

A. Specific Parking Development Standards.

1. Applicability. The following parking standards shall apply to all residential and non-residential development and/or redevelopment within the Downtown Specific Plan boundaries.
2. Compliance with these standards is not required in the following instances
 - a. a change of use within an existing building; or
 - b. an expansion of an existing building; or
 - c. a replacement of an existing building that does not result in an increase in floor area by more than 25 percent.
3. Parking requirements for uses not listed in **Table 3.10.2.A** shall be determined by a study of parking demand for that use, as prepared by a qualified professional and as accepted by the Director.
4. Uses that share a common parking facility through a contractual agreement reviewed and approved by the City Attorney shall be allowed to reduce the required number of stalls by 25 percent.
5. Uses that make parking available for public use through a contractual agreement reviewed and approved by the City Attorney shall be allowed to reduce the required number of stalls by 25 percent
6. Required parking may be located off site, provided it is:
 - a. entirely located within the boundaries of the Downtown Specific Plan;
 - b. within 1,000 feet of the property;
 - c. connected to the property by streets improved with sidewalks or walkways; and
 - d. tied to the site by a contractual agreement reviewed and approved by the City Attorney that is filed with the City and recorded with the County of Riverside.

7. On-street parking that is located directly adjacent to a development site shall be allowed to be used to satisfy minimum parking requirements and shall not be included in determining maximum surface parking allowances.
8. In lieu of providing the minimum parking required by this section, an applicant may request to pay for each required parking stall into a special fund that will be used to provide and upgrade municipal parking to serve the Downtown. The per-stall fee shall be as specified in the City's fee schedule.

B. Access.

1. Vehicular Access.
 - a. New driveways and curb cuts shall be permitted only in the DT-N Zone.
 - b. Except for the provisions in **Section 3.10.3**: where vehicle-accessible alleys are present, vehicular access to on-site parking shall be provided only via alley.
 - c. One-way driveways shall not exceed a width of 14 feet; two-way driveways shall not exceed a width of 24 feet.
 - d. Curb cuts shall not exceed the width of the driveway.
 - e. Driveways that cut across sidewalks shall not affect the slope or direction of the pedestrian zone of the public frontage (see **Section 4.3.C**).
2. Pedestrian Access. Pedestrian access must be provided from on-site parking to the alley or the public street by a path which is independent from the vehicular access.

Table 3.10.2.A Required Parking

Use Type	Min. Required	Max. allowed ²
Retail	2 / 1,000 NSF	4 / 1,000 NSF
Office	2 / 1,000 NSF	4 / 1,000 NSF
Residential	1 / DU	2 / DU
Restaurant	0.5 / 4 seats	1 / 4 seats

Notes

- 1 Net square feet
- 2 Applicable only to surface parking

3.10.3 Surface Parking

A. Configuration. See Figures 3.10.3 and 3.10.4.C.

1. No surface parking shall be provided between a façade of a primary building and parallel front or street-side property line.
2. DT-N Zone: New surface parking lots shall be allowed to abut a public street, provided:
 - a. The lot does not abut an adjacent surface parking lot.
 - b. Any portion of the surface parking lot adjacent to the street and in excess of the required driveway width is screened by a wall no greater than 4 feet, and behind a landscaped setback of a minimum of 2 feet. Any fence, wall, or railing in excess of the wall height shall be open work (as defined in Section 3.2: Glossary), and shall be no greater than 8 feet in height as measured from lowest adjacent grade.
 - c. Where abutting a street, the pavement does not exceed a width of 65'.

B. Lighting. All parking lot lighting shall comply with the standards for open space lighting in Section 3.9.1.B.2.

3.10.4 Structured Parking

A. Lining Required.

1. **At front property line.** All parking at-grade or above shall be behind at least 30 feet of occupiable space, which shall be designed per the Frontage Standards in Section 3.8. See Figures 3.10.4.A and 3.10.4.C.
2. **At side property line (corner lots).** The 30-foot-deep liner as required in Section 3.10.4.A.1 shall turn the corner and continue for at least 75 feet. This length is measured from the front of the property line as shown in Figure 3.10.4.C. The ground level for this stretch is required to comply with the Frontage Standards in Section 3.8. A 5-foot setback shall be required along non-lined portions of above-grade garages, within which is landscaping of the same quality as elsewhere on the property. See Figures 3.10.4.A and 3.10.4.C.
3. **Subterranean.** Fully-subterranean garages may extend to all lot lines. Semi-subterranean garages may extend to building faces. Figure 3.10.4.B.

Figure 3.10.3 Surface Lots

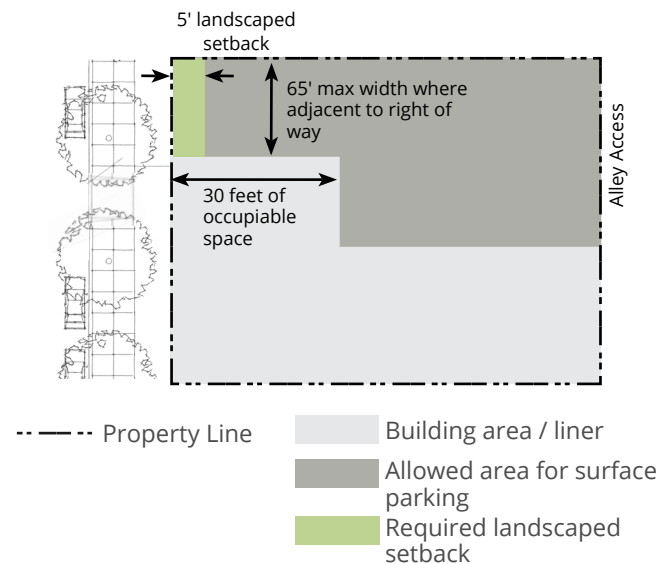


Figure 3.10.4.A Podium Parking

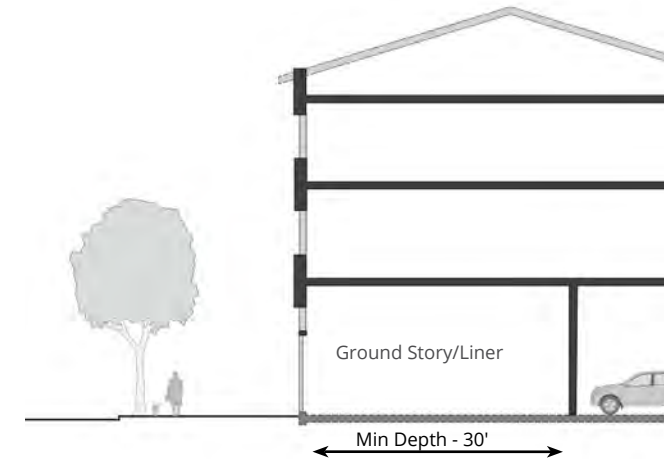
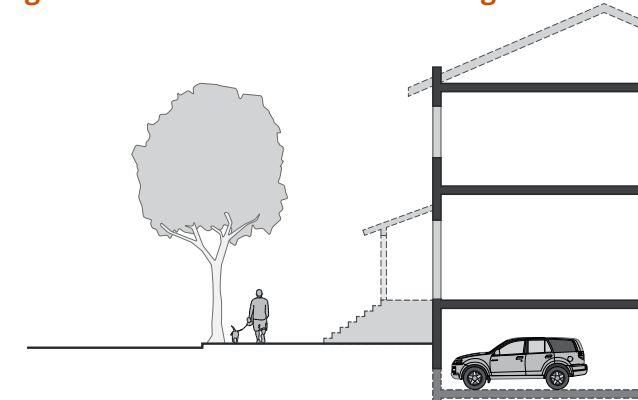
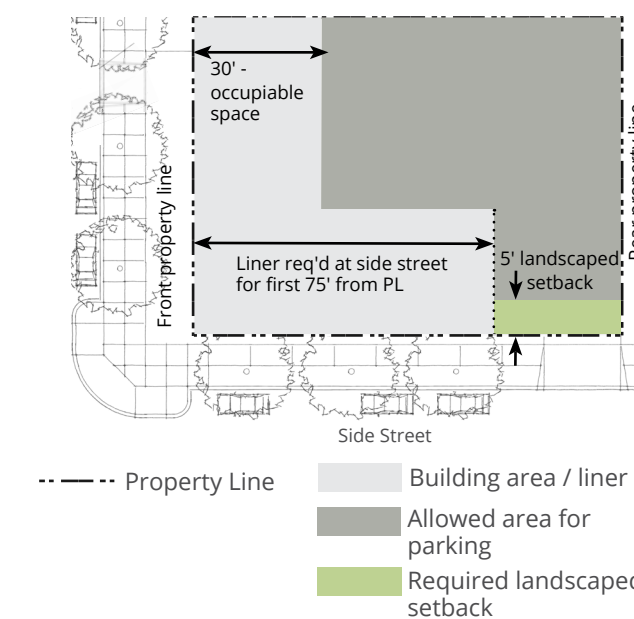


Figure 3.10.4.B Subterranean Parking



Semi-subterranean garages may extend to building faces.

Figure 3.10.4.C Parking on Corner Lots (applicable to surface and structured parking)



The non-lined portion of this structure is treated with the same quality of design as the rest of the building. Frequent and vertically-oriented windows, traditional architectural elements, and the short landscaped setback all help soften the appearance.



The vehicular entrance to this structure is as narrow as practicable, and separate from the pedestrian entrance. The attention to aesthetic detail matches that of the rest of the building.

3.11.1 Introduction

A. Applicability. Each new sign, new building, or façade renovation is subject to the standards of this section. Except where in conflict with the provisions of this section, all provisions of Chapter 150 of the Indio Municipal Code shall apply to the design and permitting of signs within the Downtown area. Any sign type that is not listed and permitted by right in this section may only be approved by the Planning Commission through Downtown Design Review. Public signs are exempt from this section.

B. General Standards.

1. Signs shall not obscure building entrances, cornices, columns, or other prominent architectural elements.
2. Allowed sign types may be combined unless stated otherwise.
3. Sign lighting shall be designed to minimize light and glare on surrounding rights-of way and properties.
4. Directory Signs. Directory signs are small wall signs located at pedestrian eye level and intended to identify multiple tenants within a building or complex. The following standards apply to these signs:
 - a. Directory signs shall not exceed 6 square feet.
 - b. Directory signs shall only be externally illuminated. Internal illumination and neon lighting is prohibited.

3.11.2 Sign Types

Table 3.11 lists the signs types that are permitted by right within the Downtown area, subject to the associated standards.

Table 3.11 Sign Types

Type	Allowance	See Section:
Wall	P	3.11.1.A
Window	P	3.11.1.B
Blade	P	3.11.1.C
Vertical Projecting	P	3.11.1.D
Front Yard	P	3.11.1.E
Monument	P	3.11.1.F
Awning	P	3.11.1.G
Sidewalk	P	3.11.1.H
Theater Marquee	DDR - Planning Commission	
Pylon	DDR - Planning Commission	
Roof	DDR - Planning Commission	

Key

- P Permitted by right
- DDR Downtown Design Review required



Wall-mounted Projecting Signs



Neon Wall Sign



Projecting Sign mounted on a shopfront's canopy



Sidewalk Signs



Wall sign painted directly on the building

A. Wall



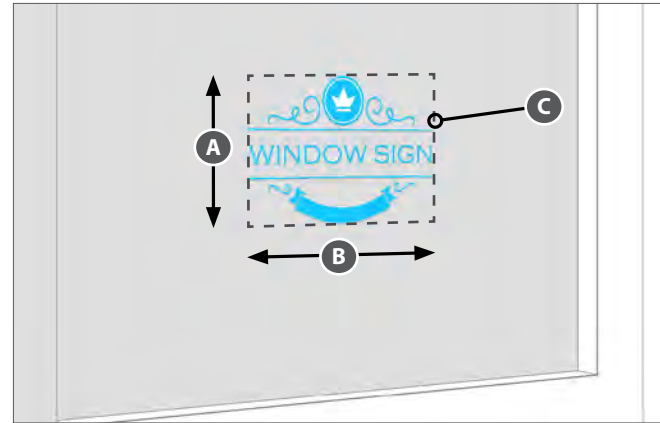
A sign that is applied directly to or projecting out and parallel to the façade. This type of sign is intended for viewing from across the street and along the sidewalk.

Standards	Min	Max
A Height	10"	30"
B Width (as % of façade width)	-	60%
C Clearance from openings	24"	-

Additional Requirements

- A minimum of one wall sign may be approved per business per street- or open-space-facing façade. In multi-tenant buildings, only the businesses with frontage on the sidewalk shall have a wall sign.
- Wall signs shall be located above the storefront and at least 12 inches from any eave or edge of building above.
- Sign thickness (as measured from the wall) shall not exceed four inches.
- Cabinet signs are prohibited.
- Channel lettering is permitted, provided that the letters are either back-lit or externally illuminated; internally-illuminated channel letters are prohibited.
- Lettering on background panel may be up to 24 inches tall. If a background panel is not included, letter height may be up to 30 inches tall.

B. Window



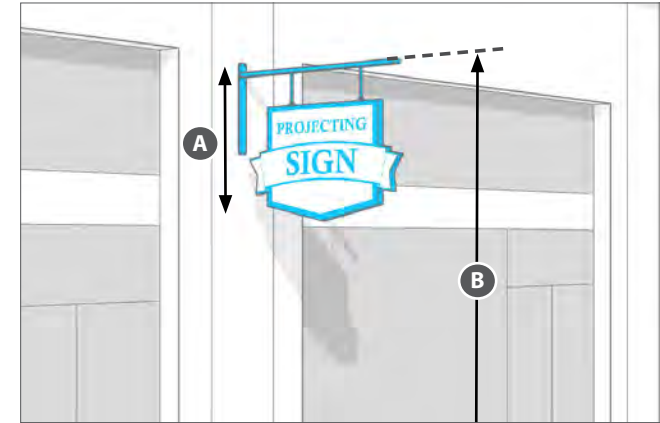
A sign painted or applied directly to the storefront windows and/or doors. Window signs also include posters for advertisements and sales, product merchandise posters, open/closed signs, and painted or etched business names and logos.

Standards	Min	Max
A Height (% of window/door height)	-	50%
B Width (% of window/door width)	-	50%
C Area (% of window/door area)	-	25%

Additional Requirements

- Permanent window signs shall be individually painted, etched or otherwise applied letters or logo graphics surrounded by clear glass.
- If no awning is provided, secondary window signs are allowed, which may span the width of the window, shall be located at the top of the window, and shall be limited to one foot in height.

C. Blade



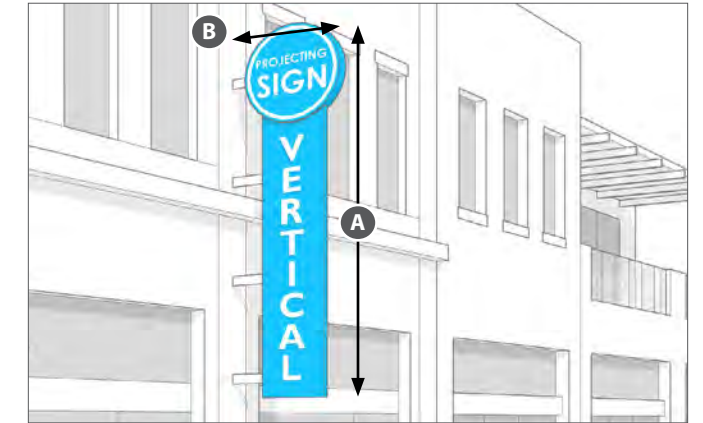
A double-sided sign perpendicular to the building façade from a mounted wall brace or from a ceiling. Blade signs typically project over a sidewalk and are intended for viewing by approaching pedestrians.

Standards	Min	Max
A Sign height	-	36"
B Vertical clearance from sidewalk	8'	12'
Area (sign only; per side)	-	6 sf
Sign thickness	-	4"

Additional Requirements

- A maximum of one blade sign shall be allowed for every storefront entrance on the façade.
- Blade signs may extend into the public R.O.W.
- The top of a blade sign shall be located below the building's second floor windows.
- Signs shall be externally illuminated by a light mounted on the façade or by neon tubing used to illuminate letters, symbols, and accent frames.

D. Vertical Projecting



A painted, reverse-channel, or individual-lettered sign applied directly to or projecting out and parallel to the façade. This type of sign is intended for viewing from across the street and along the sidewalk. Internally-illuminated cabinet signs are prohibited.

Standards	Min	Max
A Height	-	8'
B Average outer distance from façade	-	4'
C Area (sign only; per side)	-	16'

Additional Requirements

- Projecting signs shall be permitted to extend above the parapet or roof of the structure to which it is attached.
- A maximum of one projecting sign is allowed for every storefront entrance on the façade.
- Vertical projecting signs extending into the public R.O.W. require Public Works review and approval.
- Signs shall be externally illuminated by a light mounted on the façade or by neon tubing used to illuminate letters, symbols, and accent frames.

E. Front Yard Sign



A front yard sign is intended for businesses in buildings that are wholly or partially set back from the public right of way. The target viewer is the passing pedestrian or bicyclist. Front yard signs may be freestanding or integrated into the design of a fence or dooryard wall.

Standards	Min	Max
A Sign width	-	30"
B Sign height	-	30"
C Overall height	-	5'
D Horizontal clearance from sidewalk	2'	-

Additional Requirements

- Signs shall not encroach into the public right of way.
- Signs shall be oriented toward side property lines, unless incorporated into fence or wall design.
- Exterior illumination shall be permitted, provided that it is from a single bulb, shielded, and warm in tone.

F. Monument



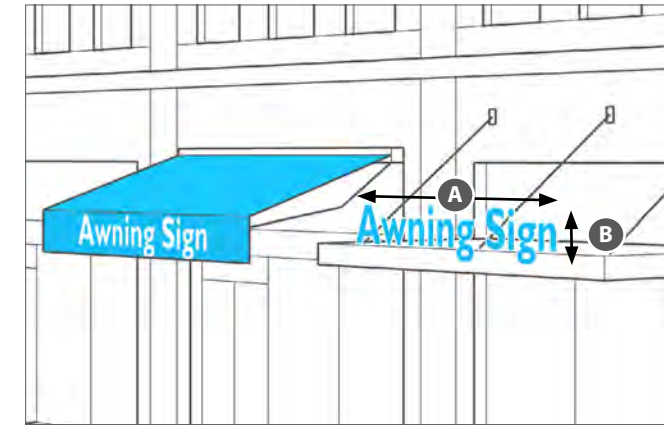
A short, solid sign that rests directly on the ground, rather than being propped up by narrower support elements.

Standards	Min	Max
A Overall sign width	-	10'
B Overall sign height	-	5'
Distance from sidewalk	2'	-

Additional Requirements

- Monument signs are only permitted along State Route 11.
- LED messages displays are prohibited.
- Marquee (changeable letter/word) displays are prohibited.

G. Awning



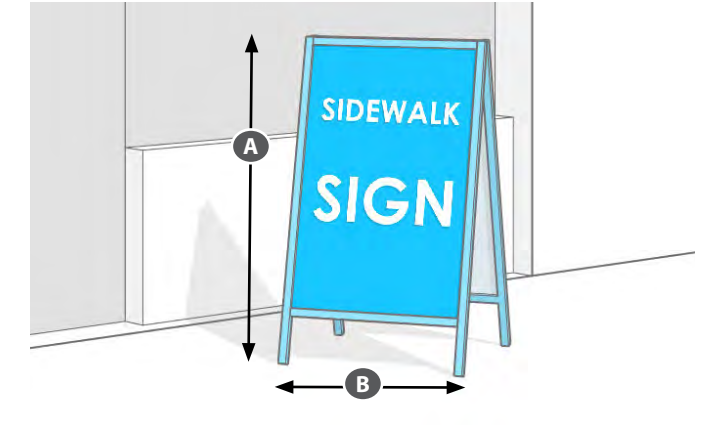
A sign painted directly onto, or projecting from any sort of awning or canopy.

Standards	Min	Max
A Letter height	10"	30"
B Width (as % of façade width)	-	60%
Area (as % of each surface)	-	25%

Additional Requirements

- If no awning is provided, secondary window signs are allowed, which may span the width of the window, shall be located at the top of the window, and shall be limited to one foot in height.

H. Sidewalk



A two-sided, non-illuminated, portable and temporary sign placed outside a storefront on the sidewalk for viewing at close range.

Standards	Min	Max
A Height (overall)	18"	36"
B Width	18"	30"
Horizontal clearance from curb	18"	-
Clear pathway	6'	-

Additional Requirements

- Signs shall not contain posters, flyers, balloons, pennants or flags attached to the sign.
- There shall be no projections other than raised carved letters, which should extend no more than ½ inch from the sign face.
- Maximum of one sidewalk sign per business.
- Signs shall only be displayed during hours of operation.
- Signs shall not be affixed to any wall or mounted on wheels.
- "Reader board" signs with removable slide-in letters are prohibited.
- Signs shall be constructed of wood or plastic with professionally-applied lettering or images, provided that handwritten menu boards or "specials of the day" for restaurants shall be allowed.