

Central Highway III Corridor Specific Plan



Prepared by:
Hogle-Ireland, Inc.

Irvine Office
2860 Michelle Drive, Suite 100
Irvine, CA 92606

Riverside Office
4280 Latham Street, Suite C
Riverside, CA 92501

Palm Springs Office
2825 E. Tahquitz Canyon Way, Suite 106
Palm Springs, CA 92262

Prepared for:
City of Indio
Community Development Department
100 Civic Center Mall
Indio, California 92202

March 2006



CENTRAL HIGHWAY III CORRIDOR
SPECIFIC PLAN

Prepared by:

Hogle-Ireland, Inc.
2860 Michelle Drive, Ste. 100
Irvine, CA 92606
949.553.1427

4280 Latham Street, Ste. C
Riverside, CA 92501
951.787.9222

2825 East Tahquitz Canyon Way, Ste. 106
Palm Springs, CA 92262
760.327.9042

[This page intentionally left blank]

TABLE OF CONTENTS

Introduction.....	1
1.1 Specific Plan.....	1
1.2 Objectives for Development of the Specific Plan.....	2
1.3 Authority for the Specific Plan.....	2
1.4 Project Location.....	3
1.5 Background.....	3
1.6 Project Setting.....	9
1.6.1 Specific Plan Site - Existing Land Uses.....	9
1.6.2 Specific Plan Vicinity - Existing Land Uses.....	9
1.6.3 Specific Plan – Planning Areas.....	9
1.6.4 General Plan Land Use Designations.....	9
2 Zoning Districts.....	15
2.1 Specific Plan Zoning Districts.....	15
2.1.1 Residential High-Density Specific Plan.....	15
2.1.2 Neighborhood Commercial- Specific Plan.....	15
2.1.3 Community Commercial-Specific Plan.....	15
2.1.4 Mixed Use-Specific Plan.....	19
3 Infrastructure.....	21
3.1 Roadway System.....	21
3.1.1 Base Year Condition.....	21
3.1.2 Future Year Improvements.....	22
3.2 Water.....	29
3.3 Wastewater (Sewer).....	29
3.4 Stormwater Run-off.....	29
3.5 Solid Waste.....	29
3.6 Utilities.....	29
3.6.1 Electricity.....	29
3.6.2 Natural Gas.....	30
3.6.3 Telephone, Internet and Cable.....	30
3.7 Infrastructure Funding & Development.....	30
4 Public Facilities and Services.....	37
4.1 Schools.....	37
4.2 Public Safety.....	37
4.2.1 Law Enforcement.....	37
4.2.2 Fire Protection.....	37
4.3 Public Transportation.....	37
5 Use and Development Standards.....	45
5.1 General Purpose.....	45
5.1.1 References.....	45
5.1.2 Resolution of Issues.....	45
5.2 Design Review and Site Plan Review.....	45
5.3 Land Uses.....	46
5.3.1 Permitted Uses.....	46
5.4 Residential Development Standards.....	48

5.5	Commercial Development Standards.....	49
5.6	Conceptual Master Plans.....	50
5.7	Mixed Use Modifications.....	54
5.8	Additional Development Standards.....	55
5.8.1	Commercial Buildings.....	55
5.8.2	Projections into Yards.....	55
5.8.3	Mechanical Equipment.....	55
5.8.4	Trash Enclosures/Refuse Bins.....	56
5.8.5	Fences and Walls.....	56
5.9	Public Amenities.....	57
5.9.1	Pedestrian Plazas and Corridors.....	57
5.9.2	Public Art.....	61
5.10	Drive-thru Facilities.....	61
5.11	Landscaping.....	61
5.11.1	Rights-of-Way.....	61
5.11.2	John Nobles Avenue Landscaped Buffer.....	61
5.11.3	Landscaped Setbacks.....	65
5.12	Vehicle Access, Off-Street Parking, and Loading.....	67
5.12.1	Purpose.....	67
5.12.2	Application.....	67
5.12.3	Vehicle Access.....	67
5.12.4	Change in Use or Intensity.....	68
5.12.5	General Development Standards.....	68
5.12.6	Required Parking.....	73
5.12.7	Off-Street Loading Requirements.....	74
5.13	Sign Regulations.....	78
5.13.1	Commercial.....	78
5.13.2	Residential.....	80
6	Design Guidelines.....	83
6.1	Design Goals.....	83
6.2	Guidelines for Development.....	83
6.2.1	Commercial.....	83
6.2.2	Residential.....	89
6.3	Site Lighting.....	91
6.4	Exterior Building Materials.....	92
7	Implementation Plan.....	109
7.1	Financing and Fees.....	109
7.2	General Provisions.....	109
7.2.1	Applicability.....	109
7.2.2	Administration.....	109
7.2.3	Interpretation.....	109
7.2.4	Implementation.....	110
7.2.5	Specific Plan Revisions.....	110
7.2.6	Minor Modifications.....	110
7.2.7	Appeals.....	111
7.2.8	Maps.....	111

7.3 General Plan Consistency	111
Appendices	113
Appendix A. Definition of Terms	115
Appendix B. General Plan Consistency	119

LIST OF EXHIBITS

Exhibit 1-1 & 1-2: Regional Context & Vicinity Maps.....	5
Exhibit 1-3: Aerial Photograph	7
Exhibit 1-4: General Plan Designations	13
Exhibit 2-1: Land Use Plan (Zoning).....	17
Exhibit 3-1: Circulation Plan.....	25
Exhibit 3-2: Street Sections	27
Exhibit 3-3: Water Lines	31
Exhibit 3-4: Sewer Lines	33
Exhibit 3-5: Gas Lines	35
Exhibit 4-1: Hoover Elementary School Sphere of Influence.....	39
Exhibit 4-2: Indio Middle School and Jefferson Middle School Sphere of Influence.....	41
Exhibit 4-3: Indio High School Sphere of Influence.....	43
Exhibit 5-1: Height Restriction Area Map.....	51
Exhibit 5-2: Plaza View	59
Exhibit 5-3: Typical Street Sections/Setbacks.....	63
Exhibit 5-4: Dimensions for Parking Facilities	69
Exhibit 5-5: Parking Lot Pedestrian Walkways	75
Exhibit 5-6: Typical Sign Locations.....	81
Exhibit 6-1: Conceptual Site Plan.....	85
Exhibit 6-2: Design Guidelines for Main Streets	93
Exhibit 6-3: Design Guidelines for "In-Line" Commercial Buildings.....	95
Exhibit 6-4: Design Guidelines for Commercial Plazas.....	97
Exhibit 6-5: Design Guidelines for "Big Box" Commercial Buildings	99
Exhibit 6-6: Design Guidelines for Mixed Use Buildings	101
Exhibit 6-7: Design Guidelines for Attached 2-3 Story Residential Buildings.....	103
Exhibit 6-8: Design Guidelines for Single Family 3-story Residential Buildings.....	105
Exhibit 6-9: Design Guidelines for Open Space	107

LIST OF TABLES

Table 1-1: General Plan Land Use Designations and Zoning Districts	11
Table 5-1: Permitted Uses	47
Table 5-2: Residential Development Standards	49
Table 5-3: Commercial Development Standards.....	49
Table 5-4: Parking Aisle Widths	68
Table 5-5: Required Parking	73
Table 5-6: Required Loading Spaces.....	78

INTRODUCTION

I.1 SPECIFIC PLAN

The Central Highway III Corridor Specific Plan provides a land use plan, zoning, development regulations, and design guidelines for a mixed-use project that includes a commercial center and multi-family residential units. The Specific Plan defines the uses that are permitted and their locations, as well as the design guidelines for buildings and related improvements on each of the sites. The Specific Plan will ensure that the area develops in a coordinated fashion with adequate consideration of infrastructure, services, public safety, and project financing.

The purpose of the Specific Plan is:

- a) To implement the goals and policies of the General Plan of the City of Indio through site-specific standards that provide direction for development;
- b) To establish new zoning districts for the planning area that recognize its unique place in Indio;
- c) To establish development standards and design principles for the planning area; and
- d) To serve as a guideline for quality development within the Specific Plan area.

Tentative, final tract and parcel map(s) or Development Plans (Site Plans, etc.) may be prepared and submitted to the City pursuant to the applicable provisions of the State of California Subdivision Map Act (Government Code Sections 66410 through 66499), the City of Indio Subdivision Ordinance, the City of Indio Municipal Code and consistent with the applicable provisions contained within the Central Highway III Corridor Specific Plan. Future development applications will be submitted for City review prior to development of the project site.

1.2 OBJECTIVES FOR DEVELOPMENT OF THE SPECIFIC PLAN

This Specific Plan represents an effort to address long term objectives, including the expansion of the City's economic base and meeting future residential housing needs.

The following objectives have been identified for the project:

- Create the opportunity for the development of a mixed-use project at the subject site with commercial and residential uses;
- Develop a profitable retail development in the eastern Coachella Valley;
- Capture an appropriate market share of the demand for retail goods and services in the eastern Coachella Valley;
- Create a gathering place for retail, commercial, dining and entertainment activity for the local community as well as for residents, businesses and visitors from throughout the area;
- Develop neighborhood-serving commercial uses for adjacent residential areas;
- Develop new ownership housing located close to neighborhood-serving commercial uses;
- Develop a unique shopping center in terms of style, goods and services in the eastern Coachella Valley;
- Increase sales tax and property tax revenues to the City; and
- Create local jobs.

1.3 AUTHORITY FOR THE SPECIFIC PLAN

Section 65450 et seq. of the Government Code sets forth basic requirements and provisions for specific plans. The Government Code specifically states:

"After the legislative body has adopted a general plan, the planning agency may, or if so directed by the legislative body, shall prepare specific plans for the systematic implementation of the general plan for all or part of the area covered by the general plan."

Section 65451 of the Government Code requires that specific plans include text and a diagram or diagrams that specify all of the following:

- The distribution, location and the extent of the uses of land, including open space within the area covered by the plan;
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;
- Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
- A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out the project;
- A statement of the relationship of the specific plan to the general plan.

The Central Highway III Corridor Specific Plan is designed to meet the requirements of the State of California Government Code and the City of Indio General Plan. The Specific Plan has been adopted by ordinance and thereby establishes the zoning regulations for the development of the project site.

1.4 PROJECT LOCATION

The City of Indio is located in eastern Riverside County in an area known as the Coachella Valley (see Exhibit 1-1, Regional Context). The Central Highway III Corridor Specific Plan project area is located in the central portion of the City and covers approximately 86 developable acres. The project site is bordered on the east by Arabia Street, on the north and west by Highway III and on the south by John Nobles Avenue (see Exhibit 1-2, Vicinity). The Indio Fashion Mall is found near the center of the project area on Highway III between Monroe Street and Rubidoux Street.

Exhibit 1-3 provides an aerial photograph depicting the project site and adjacent properties at the time the Specific Plan was prepared.

1.5 BACKGROUND

The project was undertaken as part of an ongoing effort to address the City's long-term objectives, including expanding the City's economic base and meeting current and future housing needs. The Indio Fashion Mall was built in the mid 1970's and the City intended to expand the mall in the 1980's by acquiring properties to the south; however the expansion never materialized. At the time the Specific Plan was being prepared, the area south of the mall along John Nobles Avenue was largely vacant with a small number of residential units and church properties. While the Fashion Mall was never expanded, development on surrounding properties continued to the point

where most properties on the north and south side of Highway 111 are improved with commercial structures.

In March 2005, a development moratorium was placed on the properties in the project area in order to develop a comprehensive Specific Plan.

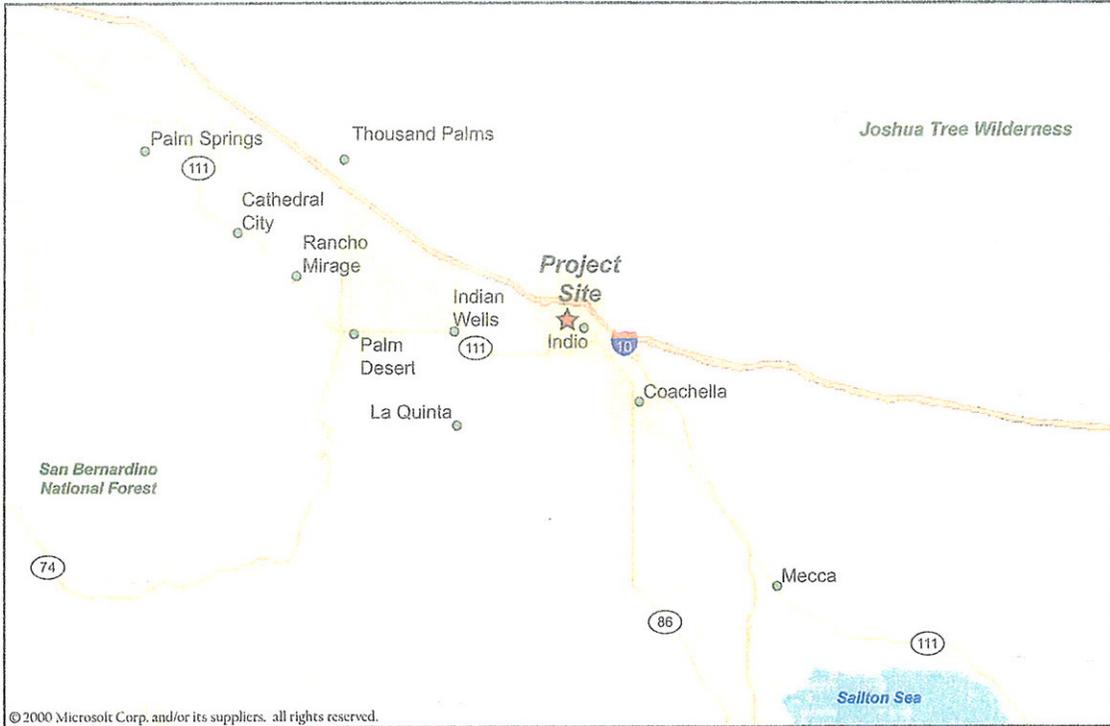


Exhibit 1-1: Regional Context Map

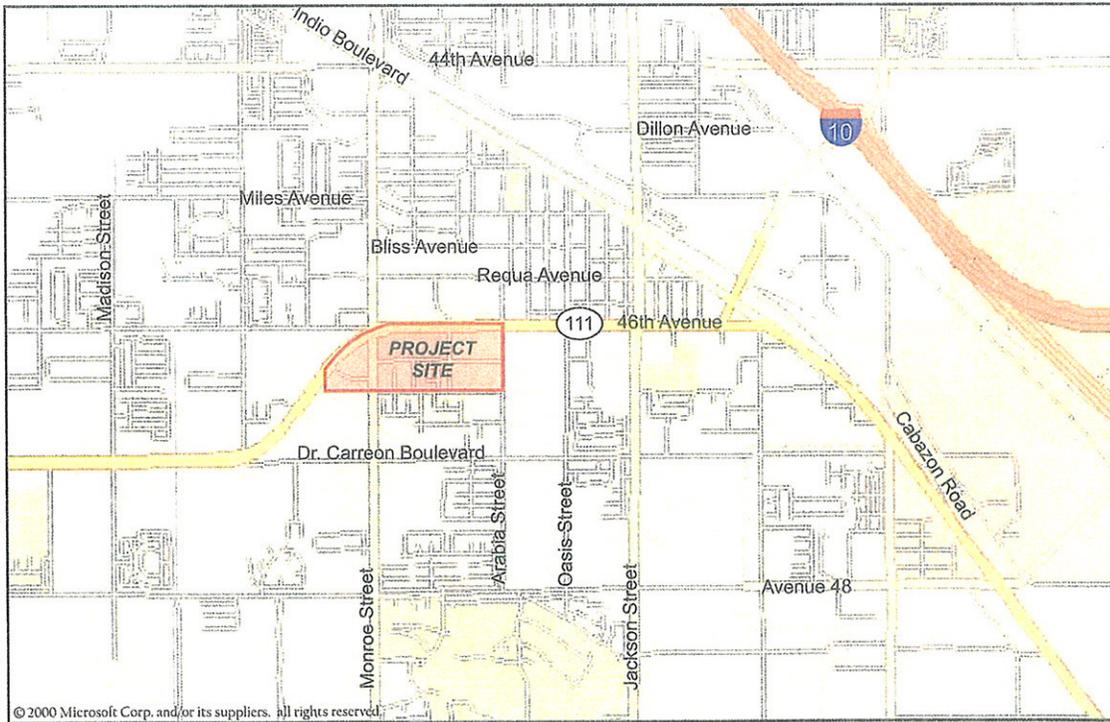


Exhibit 1-2: Vicinity Map



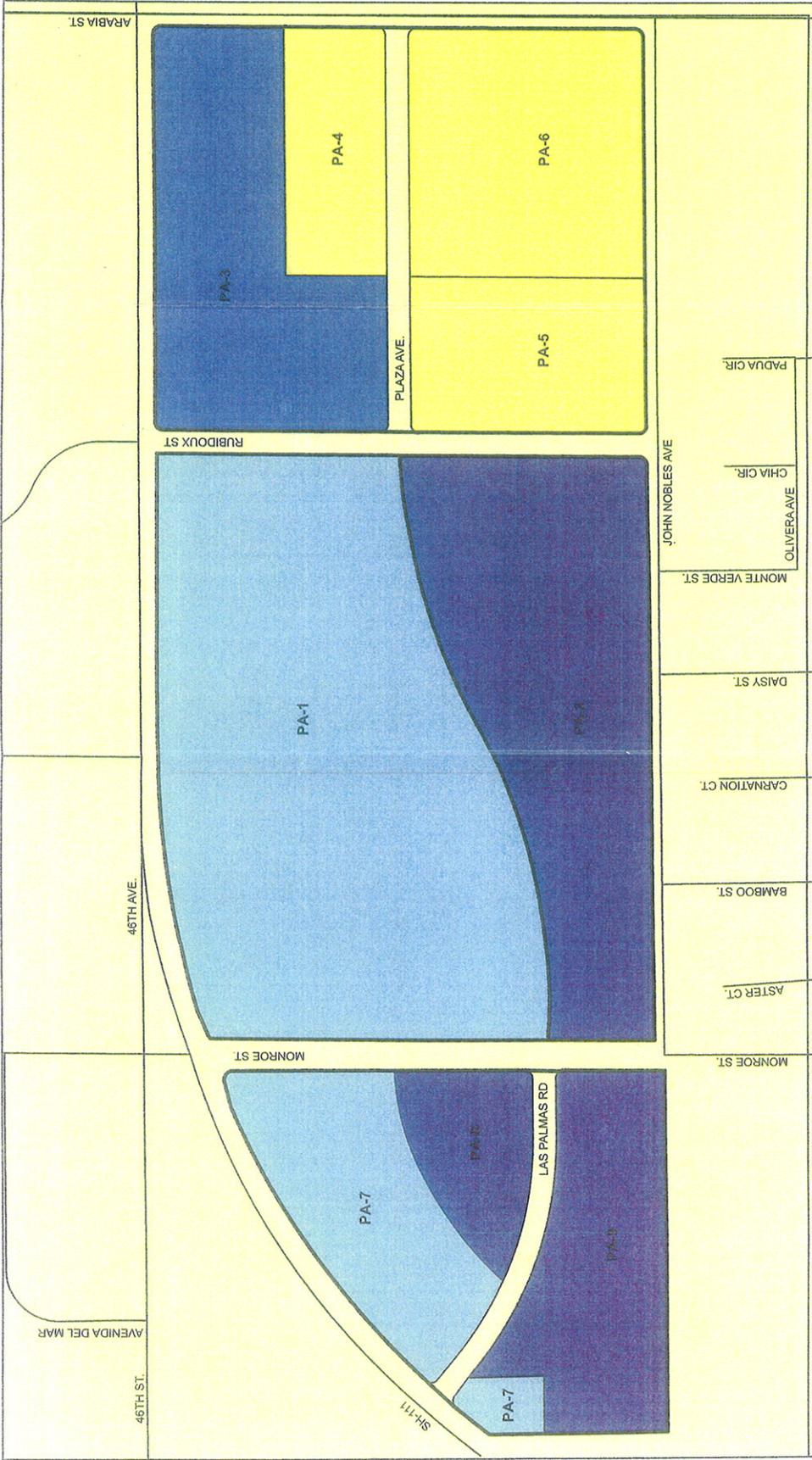




Legend

- Single Family Residential
- Multi-Family Residential
- Indio Fashion Mall
- Project Boundary





LEGEND

- Residential High-Density
- Neighborhood Commercial
- Community Commercial
- Mixed Use Specific Plan
- PA= Planning Area



1-4

General Plan Designations
Central Highway III Corridor Specific Plan

March 2006

1.6 PROJECT SETTING

1.6.1 Specific Plan Site - Existing Land Uses

The Indio Fashion Mall is located on the south side of Highway 111 between Monroe Street and Rubidoux Street. At the time the Specific Plan was developed, the area west of the Indio Fashion Mall along the south side of Highway 111 from Las Palmas Road to Monroe Street included La Pinata, Mario's and Pueblo Viejo Grill restaurants, and office and retail service uses including Century 21 Real Estate and Goodyear Tires. Areas on the south side of Highway 111 east of the Mall included a Firestone tire store, a motel and restaurant, and a low-rise office building

The Specific Plan project area located south of the Fashion Mall was largely vacant except for a few single-family dwellings and three churches. Multi-family residences are located in the block bordered by Rubidoux Street, Plaza Avenue, Arabia Street and John Nobles Avenue, and on the north side of Plaza Avenue. Multi-family residences are also located in the southwest portion of the Specific Plan project area near Highway 111 and Las Palmas Road.

1.6.2 Specific Plan Vicinity - Existing Land Uses

Development in the area around the Central Highway 111 Corridor Specific Plan site includes shopping centers anchored by grocery and drug stores and complementary convenience retail goods and service uses such as banks, restaurants, and professional offices across Highway 111 to the north and west. The Riverside County Fairgrounds are located to the east of the Specific Plan site across Arabia Street. A single-family residential neighborhood is located south of John Nobles Avenue.

1.6.3 Specific Plan – Planning Areas

The Specific Plan site is comprised of nine Planning Areas. Planning Areas were formed according to the compatibility of uses permitted within each Planning Area and the boundaries suggested by the existing roadway network. Organization by Planning Area allows for tailoring of development standards within the Specific Plan based on differing circumstances between Planning Areas. Specific Plan Planning Areas are shown in Exhibit I-4.

1.6.4 General Plan Land Use Designations

The General Plan Land Use Designations in the Specific Plan provide general direction and guidance for physical development in the City of Indio and are summarized below:

- Residential High-Density: This designation is consistent with Policy LU-1.8 of the Indio General Plan:

The Residential High designation allows multi-family developments of apartments, condominiums, and single-family attached units.

- Neighborhood Commercial: This designation is consistent with Policy LU-3.1 of the Indio General Plan:

The Neighborhood Commercial designation provides for convenient small scale shopping and personal service uses in close proximity to residential neighborhoods.

- Community Commercial: This designation is consistent with Policy LU-3.2 of the Indio General Plan:

The Community Commercial designation is intended to provide for general merchandising and retailing establishments that serve the needs of the residents of Indio. The range of services and merchandise will be greater than those found in the neighborhood centers, and the service areas will be City-wide compared to neighborhood oriented.

- Mixed Use- Specific Plan: This designation is consistent with Policy LU-10.3 of the Indio General Plan:

The Mixed Use- Specific Plan (MU-SP) designation is intended to allow for a mix of residential and commercial land uses. This designation will be used to allow persons to live close to employment opportunities, and to provide for a transition from higher intensity commercial uses to more traditional residential developments.

Table 1-1 summarizes the Land Use Designations prior to and subsequent to adoption of the Specific Plan in addition to the Zoning Districts that make up the project area. The Land Use Designations and Planning Areas are illustrated in Exhibit 1-4.

Table 1-1: General Plan Land Use Designations and Zoning Districts

Planning Area	General Plan Designation		Zoning District	
	Prior to Adoption of Specific Plan	After Adoption of Specific Plan	Prior to Adoption of Specific Plan	After Adoption of Specific Plan
Planning Area 1	Regional Commercial	Community Commercial	Community Commercial	Community Commercial-Specific Plan
Planning Area 2	Regional Commercial	Mixed Use-Specific Plan	Residential High-Density & Residential Low-Density	Mixed Use-Specific Plan
Planning Area 3	Community Commercial	Neighborhood Commercial	Community Commercial	Neighborhood Commercial-Specific Plan
Planning Area 4	Community Commercial	Residential High-Density	Residential Low-Density	Residential High-Density-Specific Plan
Planning Area 5	Residential High-Density	Residential High-Density	Residential High-Density	Residential High-Density-Specific Plan
Planning Area 6	Residential High-Density	Residential High-Density	Residential Low-Density	Residential High-Density-Specific Plan
Planning Area 7	Community Commercial	Community Commercial	Community Commercial	Community Commercial-Specific Plan
Planning Area 8	Community Commercial & Residential High-Density	Mixed Use-Specific Plan	Community Commercial & Residential Medium-Density	Mixed Use-Specific Plan
Planning Area 9	Community Commercial & Residential High-Density	Mixed Use-Specific Plan	Community Commercial & Residential Medium-Density	Mixed Use-Specific Plan

[This page intentionally left blank]

2 ZONING DISTRICTS

2.1 SPECIFIC PLAN ZONING DISTRICTS

The City of Indio Zoning Ordinance establishes various zoning districts and corresponding use and development standards that determine the intensity and character of development, the sizes and uses of buildings and land, and the relationships between differing land uses. All properties within the Specific Plan area are assigned to a zoning district as indicated on the map in Exhibit 2-1. The Specific Plan sets forth land use and development regulations for each of these zoning districts.

2.1.1 Residential High-Density Specific Plan

The Residential High-Density-Specific Plan Zoning district totals approximately 18.3 acres and is located in the southeast portion of the Specific Plan project area along Plaza Avenue, Arabia Street, John Nobles Avenue and Rubidoux Street in Planning Areas 4, 5, and 6. Residential densities between 10 and 20 dwelling units per acre are allowed.

2.1.2 Neighborhood Commercial- Specific Plan

The Neighborhood Commercial-Specific Plan Zoning District totals approximately 10.4 acres and is located in Planning Area 3 on the south side of Highway III between Rubidoux Street and Arabia Street. This district allows up to 137,600 square feet of neighborhood-serving retail goods and service uses in close proximity to residential uses. The intent is to provide a place where local residents can go to in order to satisfy their shopping and service needs.

2.1.3 Community Commercial-Specific Plan

The Community Commercial- Specific Plan Zoning district totals approximately 32.9 acres and is located in Planning Area 1 on the south side of Highway III between Monroe Street and Rubidoux Street and in Planning Area 7 on Highway III west of Monroe Street.

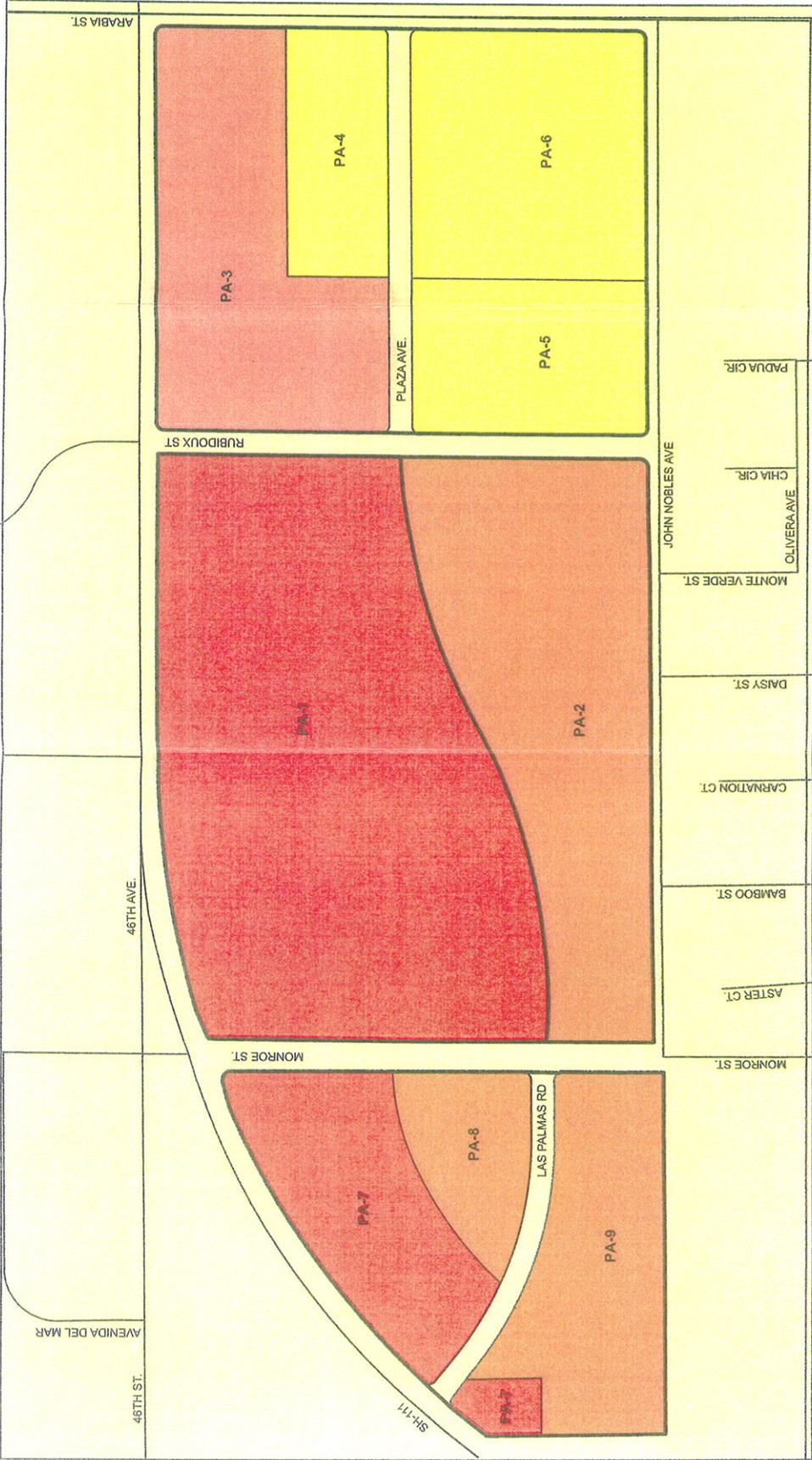
The Community Commercial-Specific Plan Zoning district encompasses up to 398,600 square feet of retail goods and services intended to serve Indio residents and visitors. In addition to retailers and service uses, restaurants and office space are included. This district will combine high visibility, ease of access, and convenient parking in order to create a community-serving shopping district.

[This page intentionally left blank]

2.1.4 Mixed Use-Specific Plan

The Mixed Use-Specific Plan Zoning District totals approximately 24.3 acres and is located in Planning Area 2 on the north side of John Nobles Avenue between Monroe Street and Rubidoux Street, in Planning Area 8 on the north side of Las Palmas Road west of Monroe Street, and in Planning Area 9 on the south side of Las Palmas Road west of Monroe Street. The Mixed Use-Specific Plan District accommodates an intermixing of residential and non-residential buildings and uses. Mixed use development may integrate residential buildings with commercial buildings, side-by-side on a single property or on adjacent properties, or vertically, most often with commercial uses as street level and residential uses on upper floors. Mixed use development encourages pedestrian activity. The proximity of residential uses and commercial uses encourages residents to walk to retail shops, restaurants and offices. The resident population provides a base for the retail component of the mixed use neighborhood. Integration of residential and non-residential uses within a neighborhood provides around the clock use and occupancy, encouraging social interaction in public areas both day and evening. Use of public spaces in mixed use neighborhoods promote a sense of ownership by residents and workers which translates into a safer environment.

[This page intentionally left blank]



LEGEND

- Residential High-Density-Specific Plan
- Neighborhood Commercial-Specific Plan
- Community Commercial-Specific Plan
- Mixed Use-Specific Plan
- P.A.-Planning Area

not to scale



Land Use Plan (Zoning)
Central Highway III Corridor Specific Plan

2-1

March 2006

3 INFRASTRUCTURE

3.1 ROADWAY SYSTEM

Highway III adjoins the northerly and westerly boundaries of the Specific Plan area. John Nobles Avenue forms the southerly boundary and Arabia Street is the easterly boundary. Monroe Street, Las Palmas Road and Plaza Avenue provide access through the interior of the Specific Plan area. The Circulation Plan for the project area is shown in Exhibit 3-1.

Roadways within the Specific Plan area are described below according to street type as defined in the Circulation Element of the Indio General Plan. Street types are defined according to anticipated traffic volumes and lane widths and configurations. A cross-section illustrating each street type is shown in Exhibit 3-2. Note that the cross-sections show each street type when fully improved.

A traffic impact analysis prepared in conjunction with the Specific Plan identified improvements to roadways that are to accompany development and redevelopment within the Specific Plan area. Streets within the Specific Plan area not fully improved at the time this Specific Plan was prepared are described under "Base Year Condition." Street improvements required for new development in the Specific Plan area are described in "Future Year Improvements."

3.1.1 Base Year Condition

Highway III – Augmented Major (B)

Defined as an Augmented Major (B) roadway in the City of Indio General Plan Circulation Element. Highway III between Monroe Street and Rubidoux is improved as a six-lane divided roadway with a raised median. Highway III generally extends east-west at the project site.

Highway III west of Monroe Street and east of Rubidoux Street is improved as a four-lane, divided roadway with a painted median providing a continuous left-turn lane.

Monroe Street – Major Arterial

Defined as a Major Arterial in the City of Indio General Plan Circulation Element, Monroe Street is a four-lane, divided roadway with a painted median that extends north-south through the westerly portion of the Specific Plan area.

Arabia Street- Collector Street

Defined as a Collector Street in the General Plan, Arabia Street is a north-south two-lane, undivided roadway at the easterly boundary of the Specific Plan area.

Las Palmas Road - Non-Classified

Las Palmas Street is a two-lane, undivided roadway trending east-west through the westerly portion of the Specific Plan area.

John Nobles Avenue - Non-Classified

John Nobles Avenue is a two-lane, undivided roadway running east-west along the southern boundary of the Specific Plan area.

Plaza Avenue - Non-Classified

Plaza Avenue is a two-lane, undivided roadway running east-west through the easterly portion of the Specific Plan area.

Rubidoux Street- Non-Classified

Rubidoux Street is a two-lane, undivided, roadway running north-south through the easterly portion of the Specific Plan area.

3.1.2 Future Year Improvements

Highway 111 - Augmented Major (B)

Highway 111 is to be fully improved to its full cross-section (from center line of roadway to right-of-way line abutting the boundary), including landscaping and parkway improvements, from the southwesterly corner of the Specific Plan area to Arabia Street.

Monroe Street - Major Arterial

Monroe Street is to be fully improved to its full cross-section, including landscaping and parkway improvements, from Highway 111 to John Nobles Avenue.

Arabia Street- Collector Street

Arabia Street is to be fully improved to its full cross-section (from center line of roadway to right-of-way line abutting the boundary), including landscaping and parkway improvements, from Monroe Street to Arabia Street

Las Palmas Road - Non-Classified

Las Palmas Road is to be fully improved to its full cross-section, including landscaping and parkway improvements, from Highway 111 to Monroe Street. Las Palmas Avenue may be reconfigured, if deemed feasible to improve circulation, to align with the extension of Plaza Drive at Monroe Street. Realignment would shift the intersection of Las Palmas Road and Monroe Street north to provide a 330' separation from the intersection of Monroe Street and John Nobles Avenue.

John Nobles Avenue – Non-Classified

John Nobles Avenue is to be fully improved to its full cross-section (from center line of roadway to right-of-way line abutting the boundary), including landscaping and parkway improvements, from Monroe Street to Arabia Street.

Plaza Avenue – Non-Classified

Plaza Avenue is to be fully improved to its full cross-section, including landscaping and parkway improvements, from Rubidoux Street to Arabia Street. A possible extension of Plaza Avenue is under consideration as part of the proposed Specific Plan, from Rubidoux Street to Monroe Street.

Rubidoux Street– Non-Classified

Rubidoux Street is to be fully improved to its full cross-section, including landscaping and parkway improvements, from Highway III to John Nobles Avenue.

Optional Access Road and Shared Parking

An access road may be added parallel to Highway III between Las Palmas Road and Monroe Street, south of the existing commercial properties fronting Highway III. The intent of the roadway is to provide access to both existing and new commercial uses in Planning Areas 7 and 8.

With the participation of owners of property in the Planning Areas, and the cooperation of the City and the Redevelopment Agency, the access road would provide additional, shared parking and vehicle entries for existing and future commercial uses. Implementation of the access road option requires consolidation of existing driveways from Highway III through shared-use, reciprocal access agreements for driveways serving contiguous properties.

[This page intentionally left blank]



LEGEND

- Augmented Major B (Highway 111)
- Collector (Arabia Street)
- Major (Monroe Street)
- Non-Classified

not to scale





LEGEND

- 8' Sewer Line
- 10' Sewer Line
- 12' Sewer Line

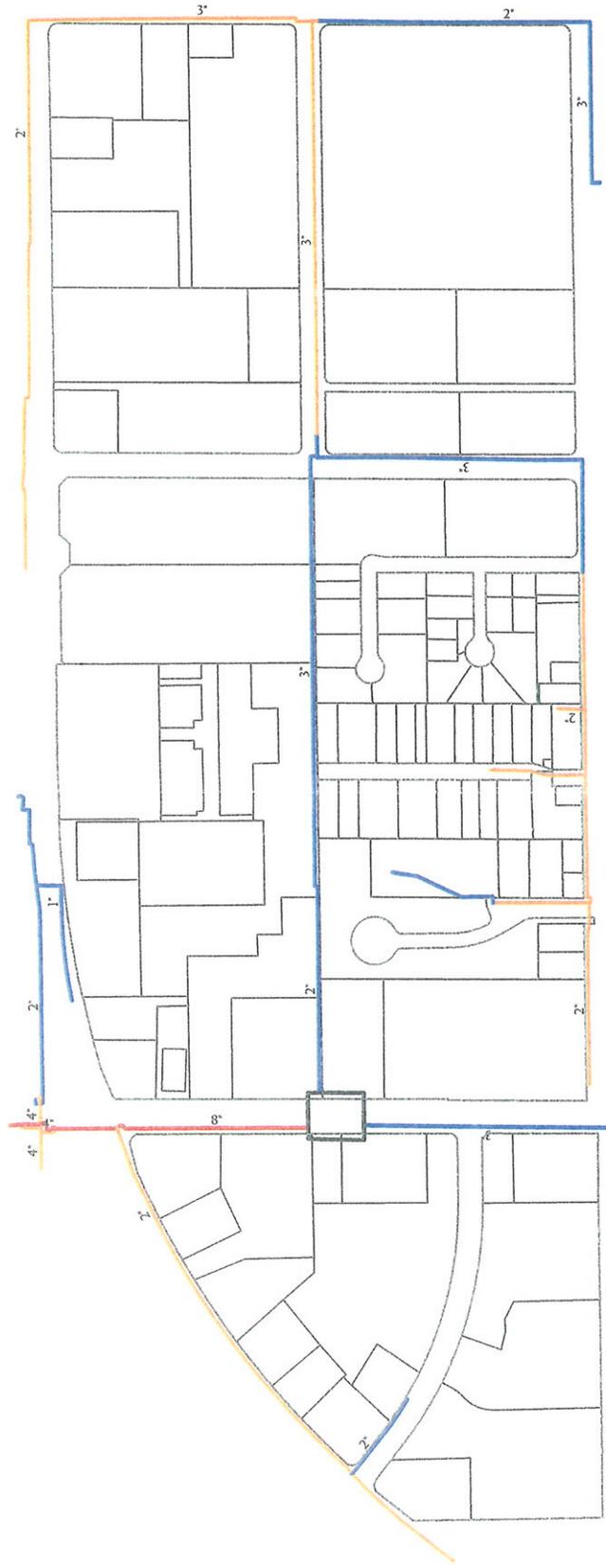
not to scale



3-4

Sewer Lines
Central Highway 111 Corridor Specific Plan

March 2006



LEGEND

- Connectors
- High Pressure
- Metal
- Plastic

not to scale



3-5

Gas Lines
Central Highway III Corridor Specific Plan

March 2006

4 PUBLIC FACILITIES AND SERVICES

4.1 SCHOOLS

The Desert Sands Unified School District serves the Specific Plan area. Students living within the Specific Plan area attend Hoover Elementary School, Jefferson Middle School or Indio Middle School, and Indio High School. Exhibits 4-1, 4-2, and 4-3 display the elementary, middle and high school spheres of influence in relation to the Specific Plan area.

4.2 PUBLIC SAFETY

4.2.1 Law Enforcement

Law enforcement for the Specific Plan area is provided by the Indio Police Department. The City of Indio Police Department employs approximately 61 sworn officers, 44 civilian employees, with an additional 50+ volunteers. New residential development within the Specific Plan project area would result in the need for one additional police officer.

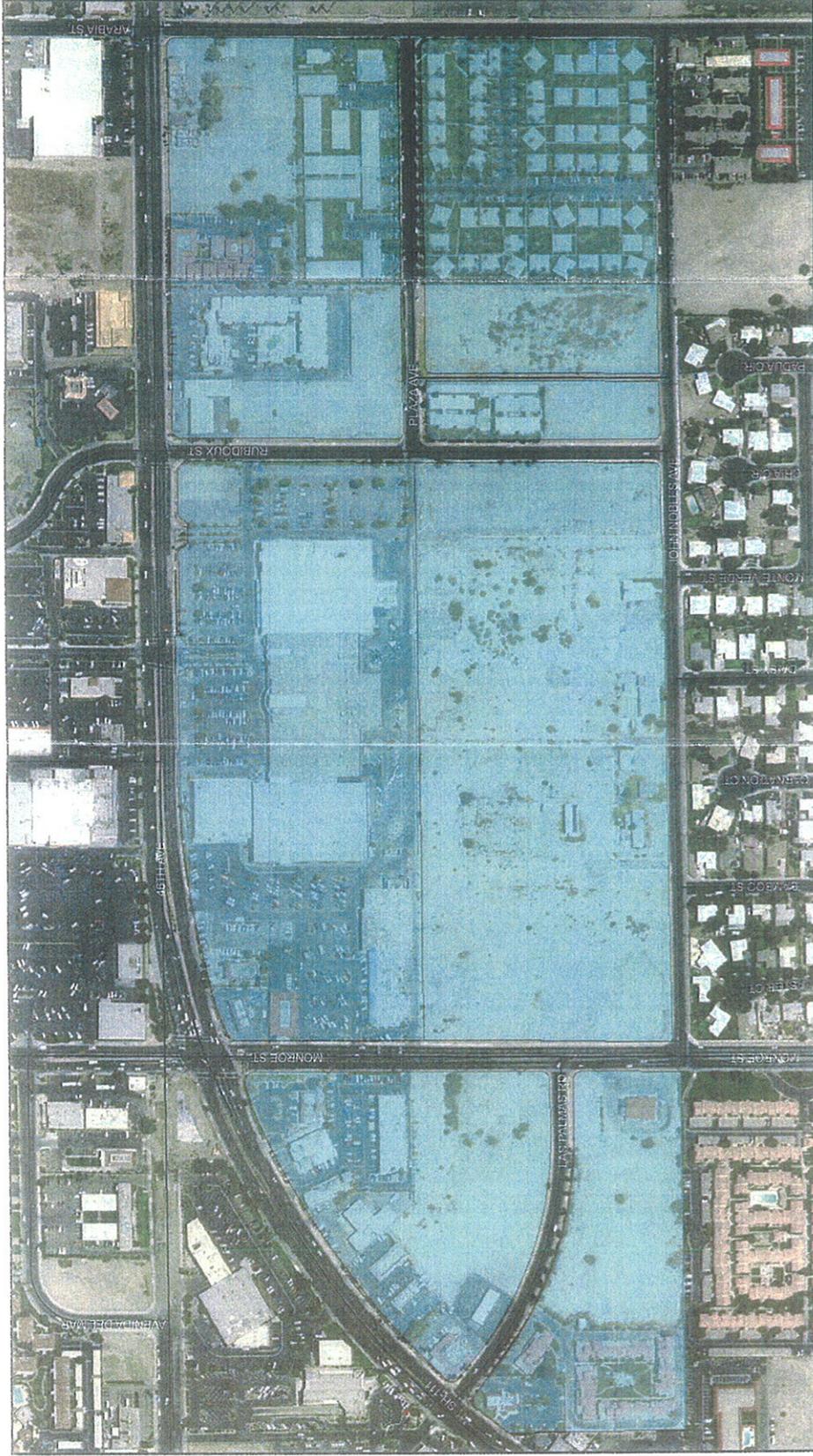
4.2.2 Fire Protection

Fire services are provided in the City as part of the regional fire protection system through a cooperative agreement with the Riverside County Fire Department. There are three existing fire stations in the City, the closest of which is located ½ mile east of the Specific Plan project area. Additional staffing needs would be addressed in the Fire Master Plan, as the City continues to grow with development in new areas or redevelopment in existing urban areas.

4.3 PUBLIC TRANSPORTATION

Sunline Transit Agency provides bus service for the Specific Plan area. Bus stop locations are determined and paid for by the transit agency. The Transit Agency has bus stops on Highway 111 within the Specific Plan project area.

[This page intentionally left blank]



LEGEND

- Hoover Elementary School
- Sphere of Influence

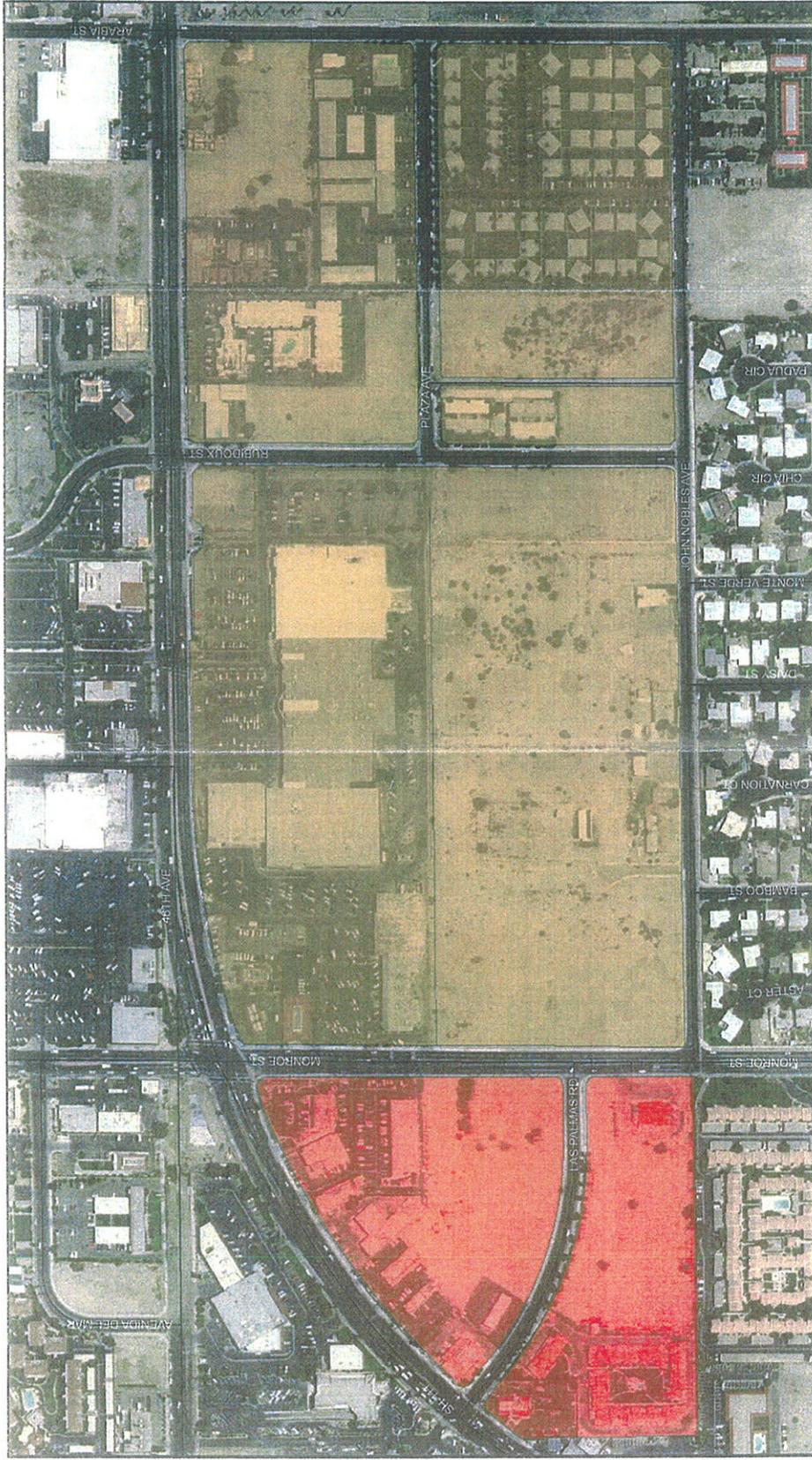
not to scale



4-1

Hoover Elementary School Sphere of Influence
Central Highway III Corridor Specific Plan

March 2006

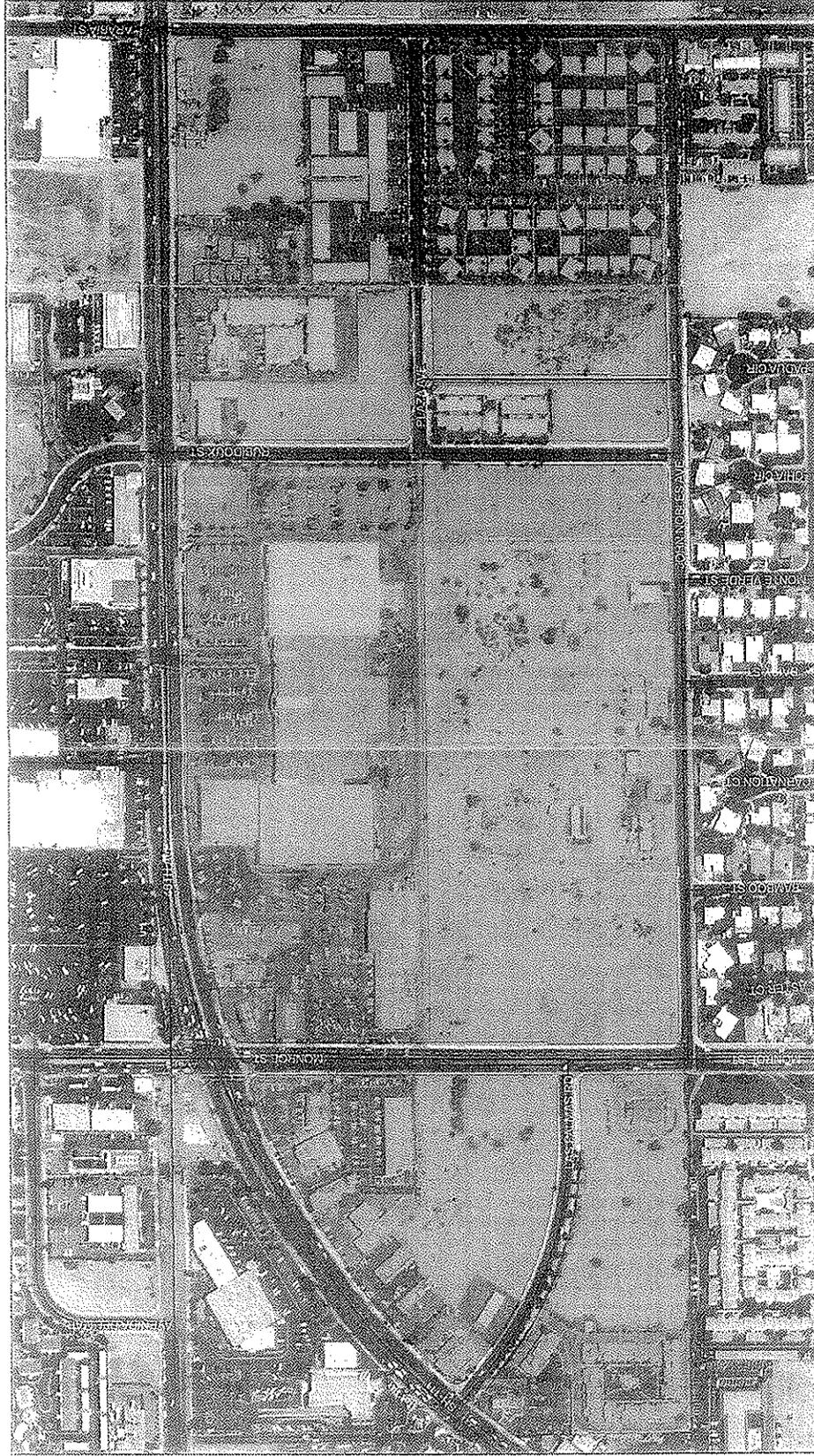


LEGEND

- Indio MS Sphere of Influence
- Jefferson MS Sphere of Influence

not to scale





LEGEND

Indio High School Sphere of Influence

not to scale



4-3

Indio High School Sphere of Influence
Central Highway III Corridor Specific Plan

March 2006

5 USE AND DEVELOPMENT STANDARDS

5.1 GENERAL PURPOSE

The Central Highway III Corridor Specific Plan Use and Development Standards define uses permitted within the Specific Plan area and regulate the sizes and locations of buildings that may house those uses and the signs for identifying them. The Use and Development Standards also provide minimum requirements for other on-site physical improvements such as parking, landscaping, sidewalks and vehicular driveways. The Use and Development Standards identify the three-dimensional framework within which the Land Use Plan for the Specific Plan will be implemented.

5.1.1 References

Any reference to Development Standards shall mean "The Central Highway III Corridor Specific Plan Use and Development Standards". Any reference to Zoning Regulations shall mean the City of Indio Zoning Regulations.

5.1.2 Resolution of Issues

- a) Whenever Specific Plan Use and Development Standards differ from or conflict with the regulations and standards contained in the Indio Zoning Regulations or any other applicable City regulation, rule or policy, the Specific Plan Use and Development Standards shall prevail unless otherwise indicated herein. The Specific Plan Use and Development Standards apply only within the area designated in the Land Use Plan for the Central Highway III Corridor Specific Plan.
- b) The Director of Community Development shall resolve any issues that may arise with interpretation of Specific Plan Use and Development Standards consistent with the purpose of this Specific Plan.

5.2 DESIGN REVIEW AND SITE PLAN REVIEW

The following development projects are subject to the Design Review and Site Plan Review process as established in Section 159.720 of the City of Indio Zoning Regulations:

- Projects requiring a parcel or tract map;
- Projects proposing new streets;
- Projects proposing an extension of an existing street;
- Projects proposing new curb-cuts;
- Projects requiring a lot line adjustment or lot merger;

- Non-residential projects with over 1,000 square feet of new construction; and
- Projects requiring review as determined by the Director of Community Development.

The Design Review and Site Plan Review process is established to meet certain community goals which are, among others, to ensure that the highest quality of land planning and design are incorporated into development projects, to ensure that new projects are compatible with existing neighborhoods in terms of scale, style and construction materials, and to ensure the maintenance, restoration, enhancement and protection of the environment.

5.3 LAND USES

The Central Highway III Corridor Specific Plan includes the map in Exhibit 2-1 that shows the locations and extent of the properties within each land use category defined in the Specific Plan. The land use categories, also referred to as zoning districts, and a summary of the general character of uses permitted within each category are identified below as follows:

- Neighborhood Commercial-Specific Plan (NC-SP): The Neighborhood Commercial-Specific Plan District provides for retail goods and service uses including frequently purchased products for consumers living or working nearby.
- Community Commercial-Specific Plan (CC-SP): The Community Commercial-Specific Plan District provides for retail goods including durable goods such as clothing and electronics, and for retail service uses including sit-down restaurants, serving customers from throughout the City and adjacent communities.
- Mixed Use-Specific Plan (MU-SP): The Mixed Use-Specific Plan District accommodates an intermixing of residential and non-residential buildings and uses.
- Residential High-Density-Specific Plan (RH-SP): The Residential High-Density District provides for multi-family dwellings at densities between 10 and 20 units per acre.

5.3.1 Permitted Uses

Permitted Uses for each Land Use Plan zoning district are identified in this section. Each use is designated as "Permitted" (Y), "Conditionally Permitted" (C), "Prohibited" (N), permitted as a "Temporary Use" (T), or permitted as an "Accessory Use" (A) to a permitted use on site in each of the four Zoning Districts.

A Permitted Use is one that can be established as the primary use of a building without need of a discretionary approval e.g. Planning Commission review provided the use complies with applicable Zoning, Building and Safety, and related State and local ordinances and policies.

A Conditionally Permitted Use requires discretionary approval e.g. a Conditional Use Permit after Planning Commission review and determination that the nature of the proposed use at the location proposed, subject to conditions of use and occupancy that may be set, can be conducted without detriment to nearby properties and uses.

A Temporary Use refers to a use of limited duration that may be permitted as a primary or accessory use of a property.

The process for application for and review of Conditional Use Permits and Temporary Permits is defined in the Indio Zoning Regulations.

Table 5-1: Permitted Uses¹

Land Use	Zoning District			
	NC-SP	CC-SP	MU-SP	RH-SP
Administrative Office	A	A	A	N
Alcohol Sales (On-site consumption)	C	C	C	N
Arcade, Game	N	C	C	N
Barber/Beauty Shop	C	Y	Y	N
Bed and Breakfast	N	N	C	C
Billboards	N	N	N	N
Business/Professional Office	C	N	Y	N
Carnival Fair, Circus	T	T	T	N
Car Wash	N	N	N	N
Check Cashing	N	N	N	N
Christmas Tree/Pumpkin Sales/ Seasonal Sales	T	T	T	T
Communications Facility (Cell Tower)	C	C	N	N
Community Center	N	N	C	C
Construction Office	T	T	T	T
Convenience Store	N	N	N	N
Day Care Center	N	N	C	N
Day Care- Home	N	N	C	C
Drive-thru Facility (Non-food)	C	C	N	N
Financial Institution	C	Y	Y	N
Fire/Police Substation	Y	Y	Y	C
Gas Station	N	N	N	N
Grocery Store (Less than 15,000 sq. ft.)	N	N	N	N
Grocery Store (Greater than 15,000 sq. ft.)	C	C	C	N
Health Club	N	N	C	N
Home occupation	N	N	Y	Y
Hospital	N	N	N	N
Hotel- Conference Center	N	C	C	N
Industrial	N	N	N	N
Liquor Store	C ²	C ²	N	N
Medical Office	N	N	N	N

Land Use	Zoning District			
	NC-SP	CC-SP	MU-SP	RH-SP
Mini-warehouse	N	N	N	N
Model Homes	N	N	T	T
Motel	N	N	N	N
Motion Picture Theater	N	C	N	N
Museum	N	N	N	N
Nightclub	N	C	N	N
Outdoor Event	T	T	T	T
Outdoor Sales	T	T	T	N
Outdoor Storage	N	N	N	N
Park, public/private	Y	Y	Y	Y
Parking Lots/Structures/Garages	A	A	A	A
Performing Arts Center	N	C	N	N
Public Library	N	Y	Y	N
Recreational Vehicle Storage	N	N	N	N
Religious Institution	N	N	N	N
Research and Development	N	N	N	N
Residential Accessory Structures	N	N	A	A
Residential, High Density	N	N	Y	Y
Residential, Low Density	N	N	Y	N
Residential, Senior Housing	N	N	Y	Y
Restaurant	C	Y	Y	N
Restaurant with drive-thru	N	N	N	N
Retail	C	Y	Y	N
School, Business/Professional	N	N	N	N
Social Club	N	N	C	N
Vehicle Leasing and Rental	N	N	N	N
Vehicle Repair	N	N	N	N
Vehicle Sales	N	N	N	N
Veterinary Clinic	N	N	C	N
Wholesale Nursery	N	N	N	N
Yard Sales	N	N	T	T

¹All uses not specified herein shall be prohibited, unless the Planning Commission determines on a case-by-case basis that a particular use is similar in nature and no more objectionable to those specified.

²Liquor for off-site consumption is a conditionally permitted use when accessory to a permitted or conditionally permitted use with floor area of 20,000 sq. ft. or more.

Y=Permitted, C=Conditional Use Permit, N= Not Permitted, T= Temporary Use Permit,
A= Accessory Use

5.4 RESIDENTIAL DEVELOPMENT STANDARDS

Table 5-2 provides a summary of standards applicable to structures and related improvements to be developed within the Residential High-Density-Specific Plan (RH-SP) and Mixed Use-Specific Plan (MU-SP) districts of the Specific Plan area.

Table 5-2: Residential Development Standards

Development Standards	RH-SP	MU-SP
Maximum Permitted Density	1 dwelling/2,150 sq. ft.	1 dwelling/2,150 sq. ft.
Minimum Development Site	40,000 sq. ft.	40,000 sq. ft.
Maximum Building Height – Main Structure	35'	35' ¹
Maximum Building Height – Accessory Structure	15'	15'
Minimum Front Setback ² – John Nobles Ave.	30'	30'
Minimum Front Setback ² – Other Streets	10'	10'
Minimum Rear Setback	15'	15'
Minimum Side Setback	15'	15'
Minimum Side Setback – Corner Lot Streetside	10'	10'
Minimum Rear/Side Setback to Garage Door	20'	20'
Private Open Space ³	250 sq. ft.	150 sq. ft.

¹ Maximum permissible building height shall be eighteen (18) feet in the Height Restriction Area shown in Exhibit 5-1. The northerly boundary of the Height Restriction Area shall be parallel to John Nobles Avenue and located 150' from the northerly property line of the residential properties on the south side of John Nobles Avenue.

² Porches, patios, and balconies may project up to 5' into the required yard.

³ Minimum 10' in width.

5.5 COMMERCIAL DEVELOPMENT STANDARDS

Table 5-3 provides a summary of standards applicable to structures and related improvements to be developed within the Neighborhood Commercial-Specific Plan (NC-SP), Community Commercial-Specific Plan (CC-SP) and the Mixed Use-Specific Plan (MU-SP) districts of the Specific Plan area.

Table 5-3: Commercial Development Standards¹

Development Standards	Commercial Zoning Districts		
	NC-SP	CC-SP	MU-SP
Minimum Development Site	40,000 sf	40,000 sf	40,000 sf
Maximum Building Height	35' ²	45'	35' ³
Minimum Streetside Setback ⁴ - Highway 111	25' ⁵	8'	N/A
Minimum Streetside Setback ⁴ - Monroe St.	N/A	20' ⁵	20' ⁵
Minimum Streetside Setback - John Nobles Ave.	N/A	N/A	30'
Minimum Streetside Setback ⁴ - Other streets	10'	10'	10'
Minimum Setback Abutting Residential District	10'	20'	N/A
Minimum Side Setback	10'	10'	10'
Minimum Rear Setback	-0-	-0-	-0-
Parking Area Streetside Setback - Highway 111	25'	8'	N/A
Parking Area Streetside Setback - Monroe St.	N/A	20'	20'
Parking Area Streetside Setback - John Nobles Ave.	N/A	N/A	30'
Parking Area Streetside Setback - Other streets	10'	10'	10'
Parking Area Interior Setback	10'	10'	10'
See Section 5.8 for additional standards			

¹ Parcels created in shopping centers are exempt from the site development standards stated herein, as they relate to minimum site area, as long as a conceptual development plan for the entire center has been approved, and if appropriate easements for reciprocal access and parking and maintenance are provided.

² Maximum permissible building height shall be eighteen (18) feet for all portions of a building within fifty (50) feet of a residential zone except that building height up to twenty-five (25) feet may be permitted upon approval of a Conditional Use Permit.

³ Maximum permissible building height shall be eighteen (18) feet in the Height Restriction Area shown in Exhibit 5-1. The northerly boundary of the Height Restriction Area shall be parallel to John Nobles Avenue and located 150' from the northerly property line of the residential properties on the south side of John Nobles Avenue.

⁴ For drive-thru service windows, see Section 5.10

⁵ Properties that provide a pedestrian plaza as described in Section 5.9.1 shall provide a 15' setback.

5.6 CONCEPTUAL MASTER PLANS

The Specific Plan promotes establishment of compatible and complementary uses and facilitates development and redevelopment of properties so as to be functionally and visually integrated with one another. Because there are numerous property owners in the Central Highway 111 Corridor Specific Plan area, development requiring Design Review and Site Plan Review must also be accompanied by a Conceptual Master Plan. The Conceptual Master Plan will ensure that:

- Proposed development is consistent with the community's vision for Indio;
- Proposed development is in conformance with established guidelines;
- Development does not precede construction of appropriate infrastructure improvements;
- Opportunities for cost sharing of improvements among property owners are identified;
- Joint-use and dual use facilities are developed wherever feasible.

The Conceptual Master Plan will reflect the proposed long-term, full development of the site, and include the following information:

- Scale, date and legend;
- Acreage of site;
- Site boundary lines;
- All uses, facilities and structures proposed for the site;
- Existing structures that are proposed to remain on the site and those that are to be removed;
- Utilities Plan (shall show all present utilities including sanitary, electric and water mains);
- Landscaping;
- Phasing;
- Existing development on adjoining properties;

- Arrangement of proposed uses on site;
- Existing and proposed rights-of-way for vehicular access to the property;
- Locations of vehicular access to the site;
- Locations of on-site parking and driveways;
- Locations of pedestrian walkways;
- Locations of existing and proposed walls and fences;
- Locations and configurations of proposed vehicular access to the site;
- Locations and configurations of proposed vehicular access and on-site parking and circulation within 50 feet of the project site on adjacent properties;
- Locations of parking and driveways on adjacent properties.

Each Conceptual Master Plan will facilitate determination by the City, in cooperation with each project proponent and owners of adjacent properties, of opportunities and requirements for implementing improvements indicated in the Specific Plan as well as determining the optimal location of buildings and vehicle access points in relation to adjacent properties. A Conceptual Master Plan will allow for identification and implementation of shared-use vehicular access and parking opportunities that will benefit the site proposed for development as well as adjacent properties.

Conditions of project approval will be based on the Conceptual Master Plan to ensure that proposed uses and building locations will not negatively impact surrounding properties, and that costs for public right-of-way improvements, and for improvements on private property where shared access and parking configurations are agreed to by adjacent property owners, are allocated proportionate to benefit received. Additional information may be required for a Conceptual Master Plan as needed to support project approval findings as follows:

- a) Building location and orientation complements improvements on adjacent properties and avoids negative impacts;
- b) Vehicular and pedestrian improvements are compatible with adjacent properties and maximize opportunities for shared parking and access;
- c) Landscape improvements will optimize the visual impact of open areas and contribute to a unified and attractive streetscape;
- d) Parking, loading, storage, and service areas are appropriately screened by building placement and/or landscaping
- d) Improvements on private property will not conflict with potential right-of-way improvements and the placement of future roadway realignments;
- g) Development will consolidate and minimize vehicular access along public rights-of-way to the extent feasible.

5.7 MIXED USE MODIFICATIONS

Regulations for Planning Areas 2, 8, and 9 allow the Specific Plan land uses to be adapted to changed market demands. This may be accomplished by modifying the amount of land allocated for each use category. Allocation shall be based on the number of vehicle trips assigned to each of the Planning Areas in the Traffic Impact Analysis in the Environmental Impact Report. Allocation of land areas on this basis will ensure that a Level of Service (LOS) "D" or better is maintained at roadway intersections within the plan area.

Applying the trip generation rates for land uses established by the Institute of Transportation Engineers¹ and the number of projected vehicle trips attributed to each of the Planning Areas in the Environmental Impact Report, a maximum number of peak hour trips has been calculated for and allocated to the planning areas as set forth below:

Trip Budget Allocation for Development Projects in Planning Area 2:

480 Peak Hour Trips

Trip Budget Allocation for Development Projects in Planning Areas 8 and 9:

141 Peak Hour Trips

The trip budget for Planning Area 2 may be allocated for permitted land uses anywhere within Planning Area 2 but shall not be transferred to any other Planning Area. The trip budgets for Planning Areas 8 and 9 may be allocated for permitted land uses within both Planning Areas 8 and 9 but shall not be transferred to any other Planning Area.

Allocation of land uses shall be based on the maximum permissible development intensity that could be accommodated by the proposed combination of land uses and the number of peak hour trips generated using the following formula:

Development Unit Equivalent	Peak Hour Trips
1 Multi-family dwelling unit	0.58
1 Single-family dwelling unit	1.01
1,000 square feet of Commercial/Retail	3.75
1,000 square feet of Office	1.55

For example, a proposal for 50 multi-family units and 25,000 square feet of retail space would generate 123 peak hour trips ($50 \times 0.58 + 25 \times 3.75 = 122.75$). The remaining development potential would be calculated by determining the

¹ *Trip Generation*, 7th Edition, 2003

remaining Trip Budget Allocation and applying the Development Unit Equivalent formula to the proposed development.

Applicants shall be permitted to develop only upon verification that trip generation impacts created do not exceed the Trip Budget Allocation for the area.

5.8 ADDITIONAL DEVELOPMENT STANDARDS

5.8.1 Commercial Buildings

Commercial buildings within the Specific Plan area shall be designed and constructed consistent with the following requirements:

- a) A minimum of 20% of the area of building walls adjacent to or within ten feet of required streetside setbacks shall be window glazing.
- b) Building walls adjacent to or within ten feet of required streetside setbacks and generally parallel to the public right-of-way shall be no longer than 150'.
- c) Adjacent buildings at or within ten feet of required streetside setbacks shall be separated by a minimum of 15 feet.
- d) Horizontal variations (jogs) of at least 30" between planes of adjacent wall sections shall be used to articulate building walls
- e) A minimum of 75% of the length of pedestrian walkways adjoining commercial buildings shall be covered with awnings or similar structures for shade.

5.8.2 Projections into Yards

In all zoning districts, eaves, awnings, louvers and similar shading devices; sills, belt courses, cornices, and similar features, and flues and chimney may project four feet into a required street setback and two feet into a required interior yard provided a three foot setback to property lines is maintained.

5.8.3 Mechanical Equipment

- a) All ground mounted mechanical equipment shall be completely screened from view from adjacent properties and shall be setback a minimum of three feet from side and rear property lines. Mechanical equipment is not permitted in yards abutting the public right-of-way.
- b) Air conditioners, heating, cooling and ventilating equipment and all other mechanical, lighting or electrical devices shall not disturb neighboring occupants, and shall be screened, shielded and/or sound buffered from adjacent properties.

- c) Roof mounted equipment shall be screened by a parapet wall and shall not be visible from the public right-of-way.
- d) Above ground utility boxes, telephone boxes, water lines, backflow preventers, cable boxes or similar structure within public view shall be screened to blend into surrounding areas.

5.8.4 Trash Enclosures/Refuse Bins

- a) Enclosures shall be required for refuse bins and shall be screened from view from the public right-of-way.
- b) Enclosures shall consist of six (6) foot-high split face or colored precision block masonry walls, an open-trellis cover, and solid, view-obscuring gates with self-closing devices.
- c) Enclosures are not permitted within any required streetside setback.

5.8.5 Fences and Walls

5.8.5.1 Maximum Height within Required Front Yards

Residential

- Walls or fences shall not exceed 36 inches in height in any required front yard setback.

Commercial

- Walls or fences are not permitted in the required street frontage setback areas.

5.8.5.2 Maximum Height within Required Side and Rear Yards

Residential

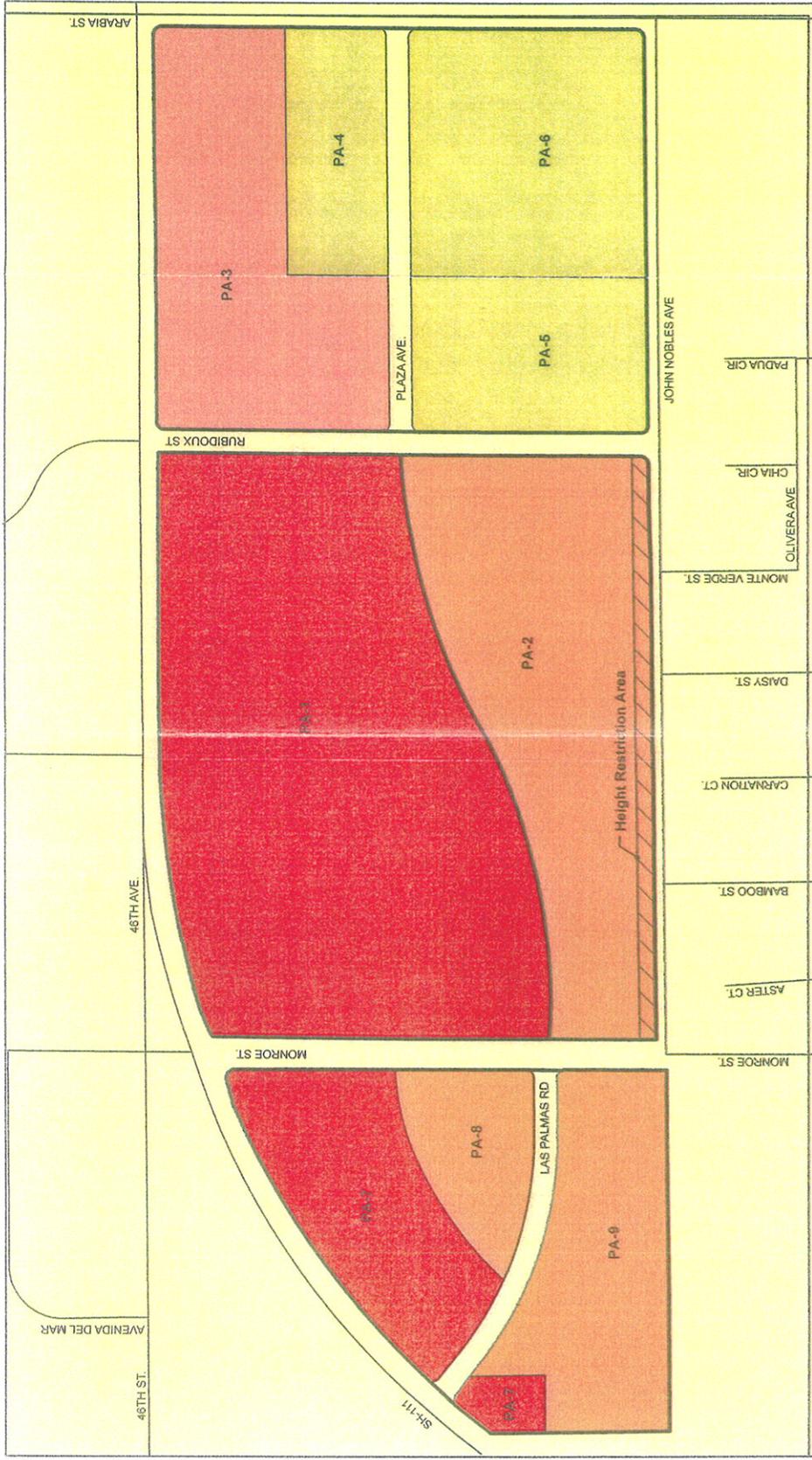
- No wall or fence shall exceed six feet.

Commercial

- Walls and fences between properties are discouraged in order to allow pedestrian access. Proposed locations shall be reviewed and approved by the Community Development Director and shall not exceed eight feet. Screen walls of greater height may be required when visual impacts such as loading zones must be mitigated.

5.8.5.3 Prohibited Wall and Fence Materials

Fiberglass sheeting, bamboo sheeting, barbed wire, razor ribbon, or other similar material shall be prohibited as fencing materials unless as otherwise approved by the Community Development Director.

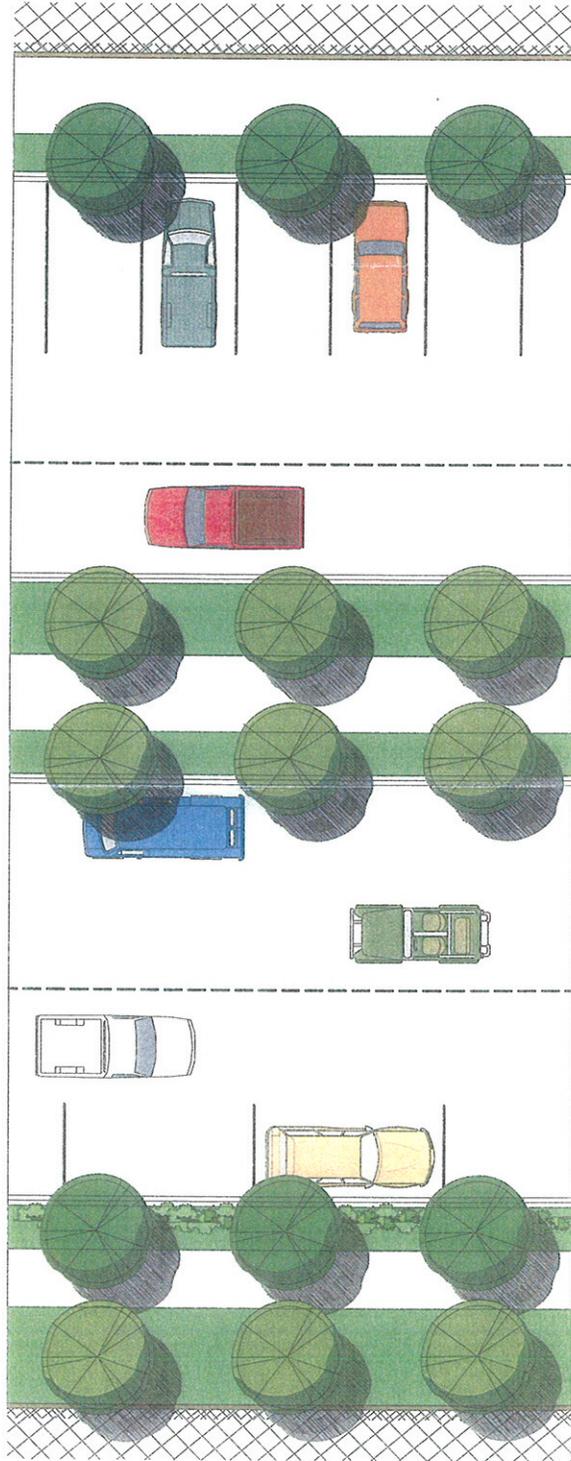
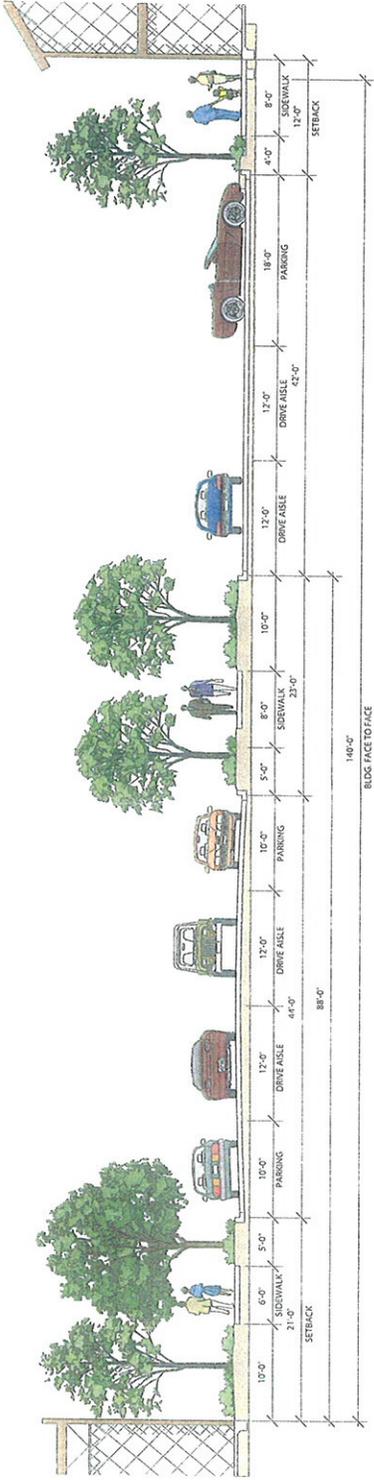


LEGEND

▨ Height Restriction Area

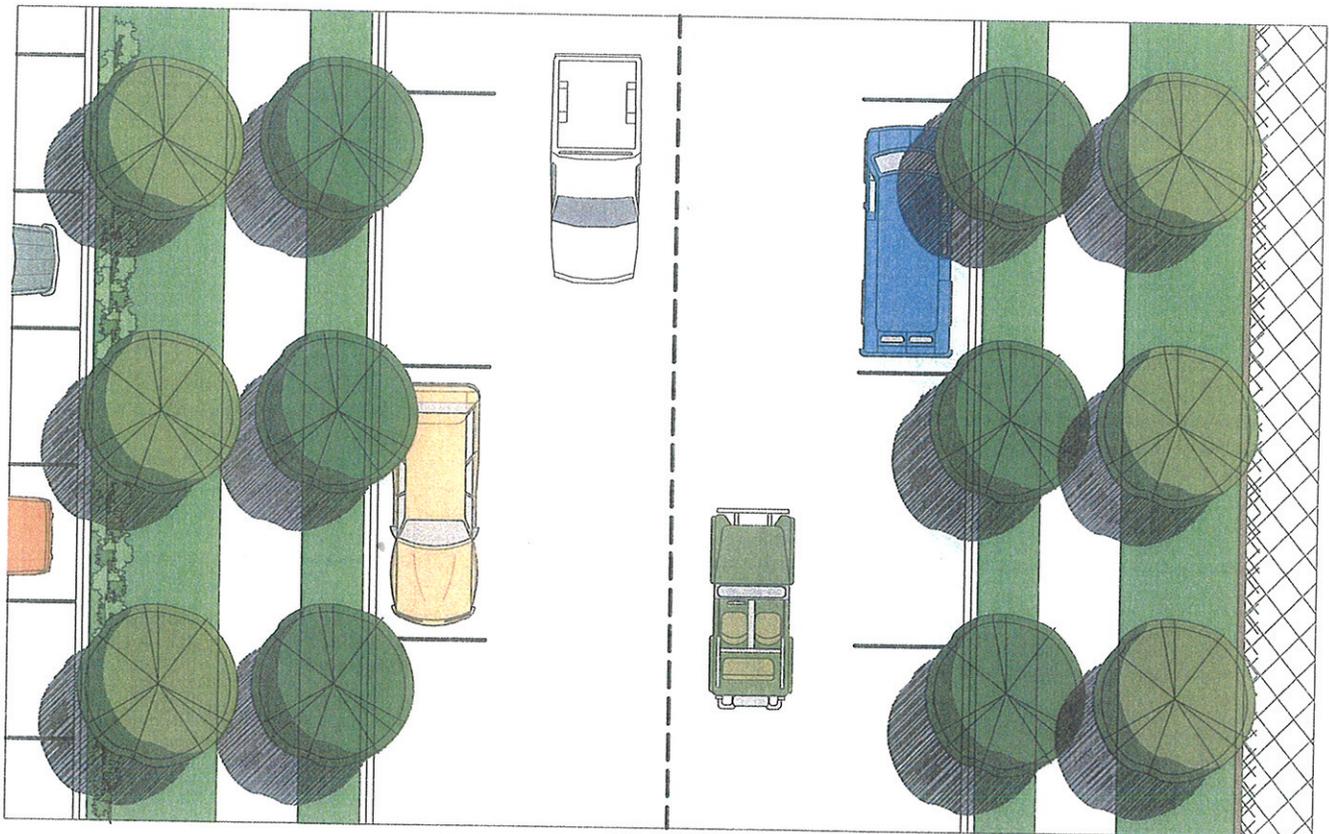
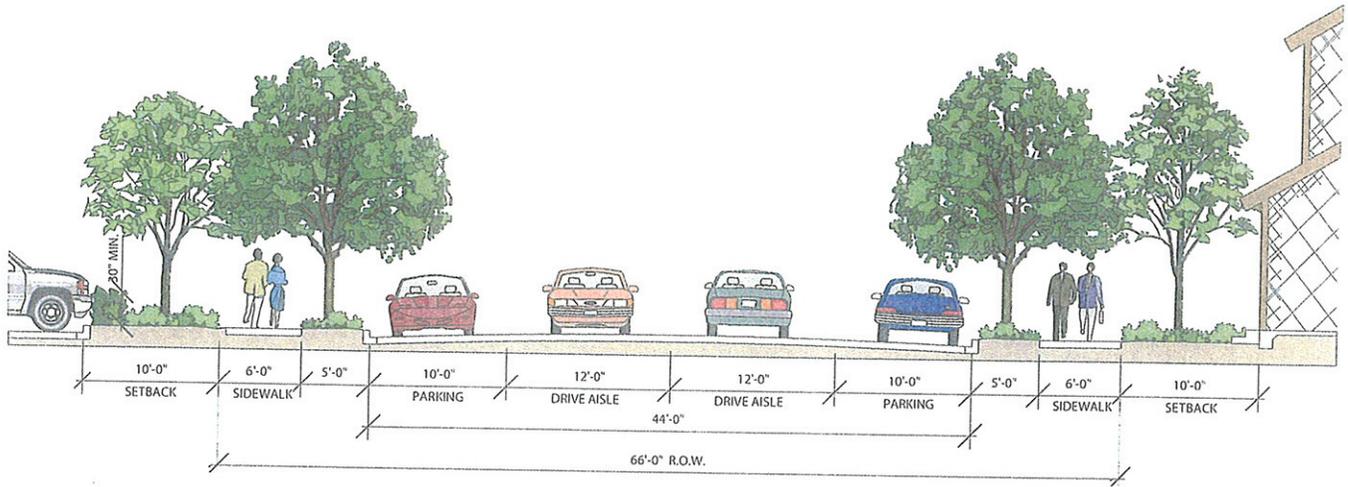
not to scale





Section-E
 Typical Street Section at Plaza Avenue Extension
 Central Highway III Corridor Specific Plan
 5-3
 March 2006

not to scale

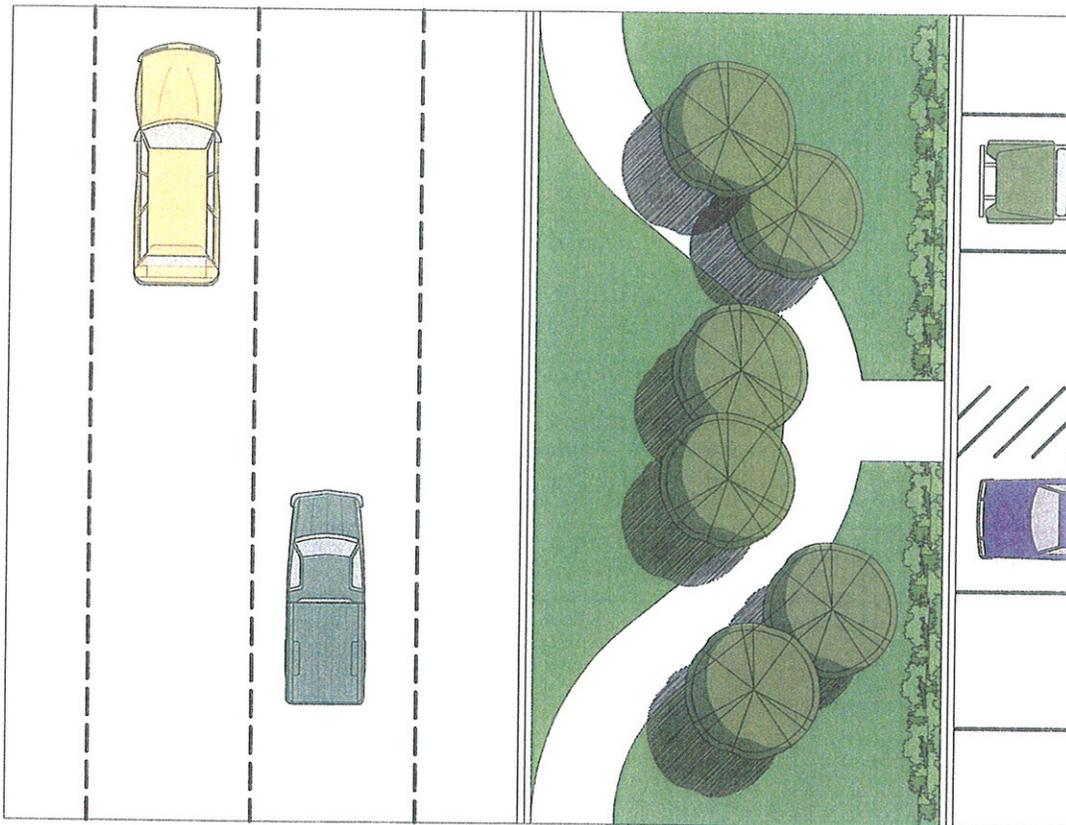
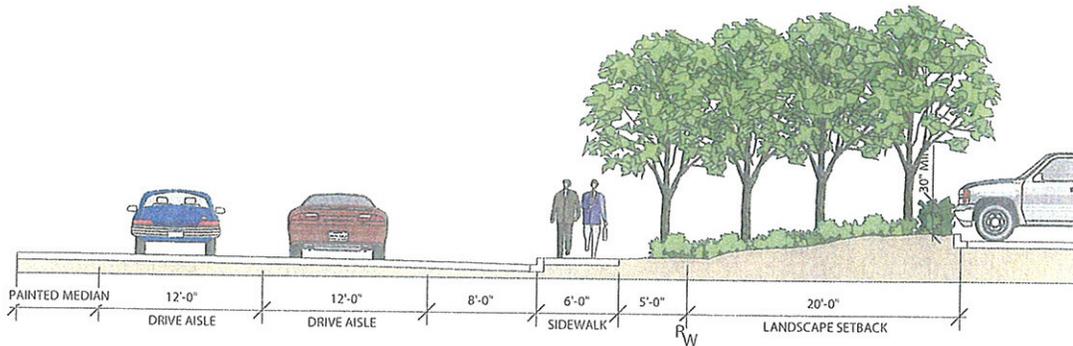


Section-B
Typical Street Section Looking East along Plaza Avenue
Central Highway III Corridor Specific Plan

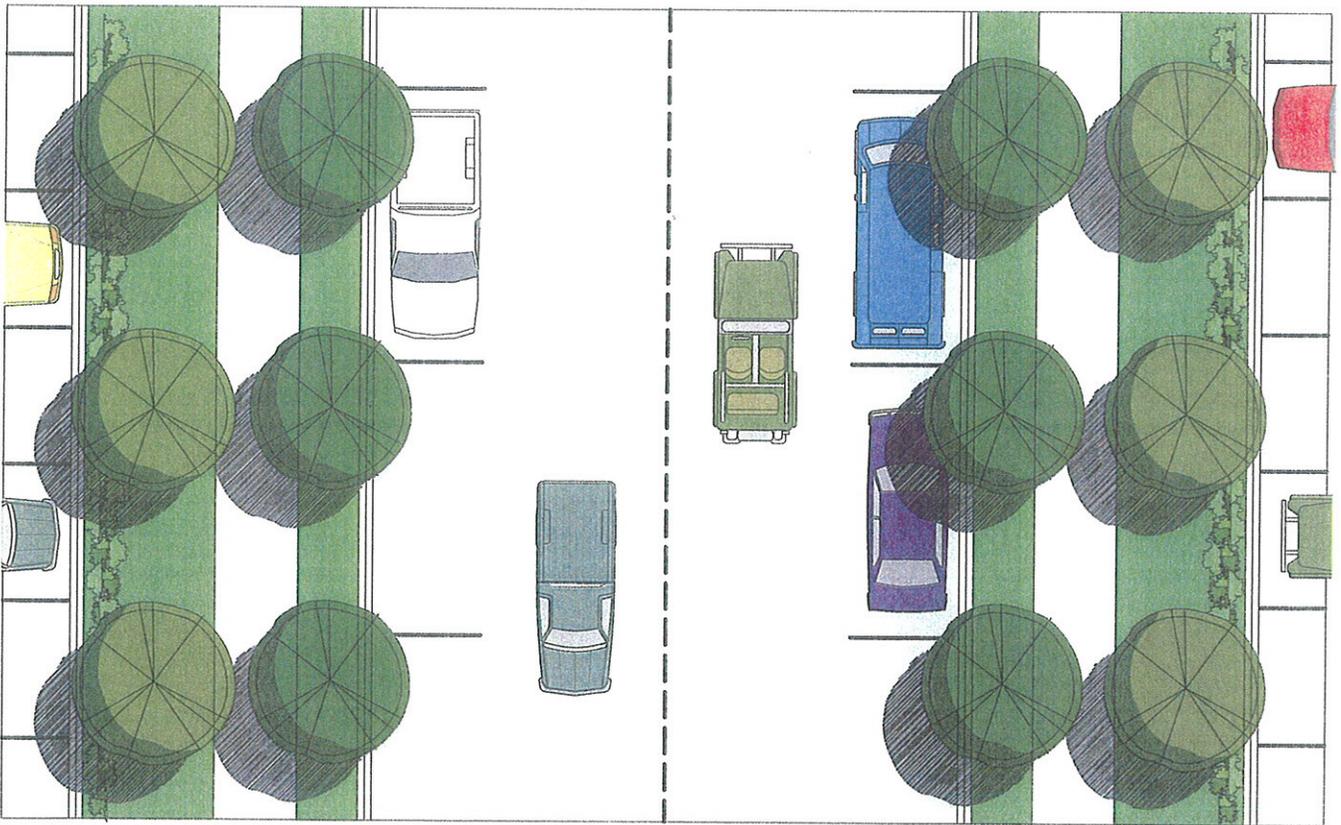
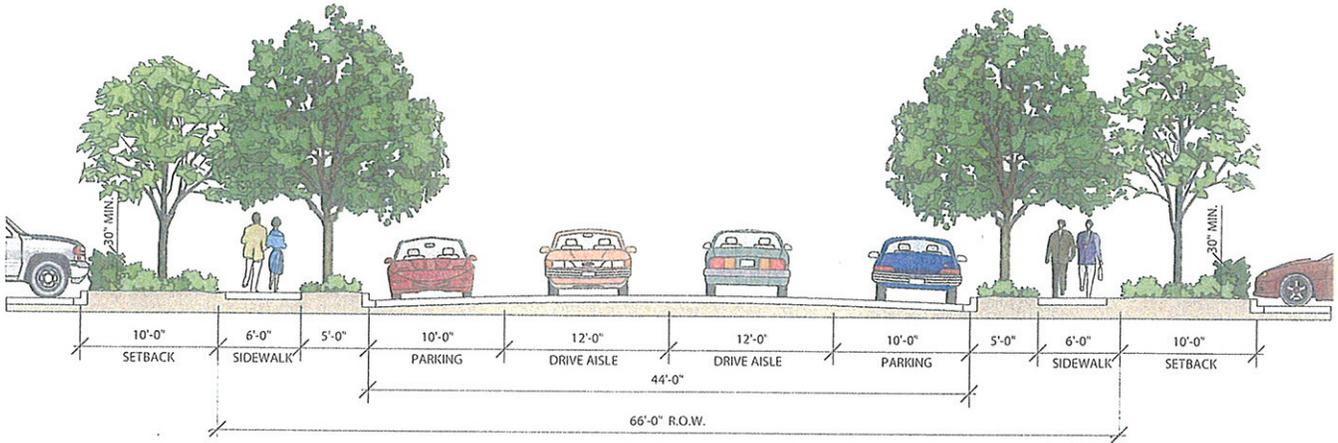
5-3

not to scale

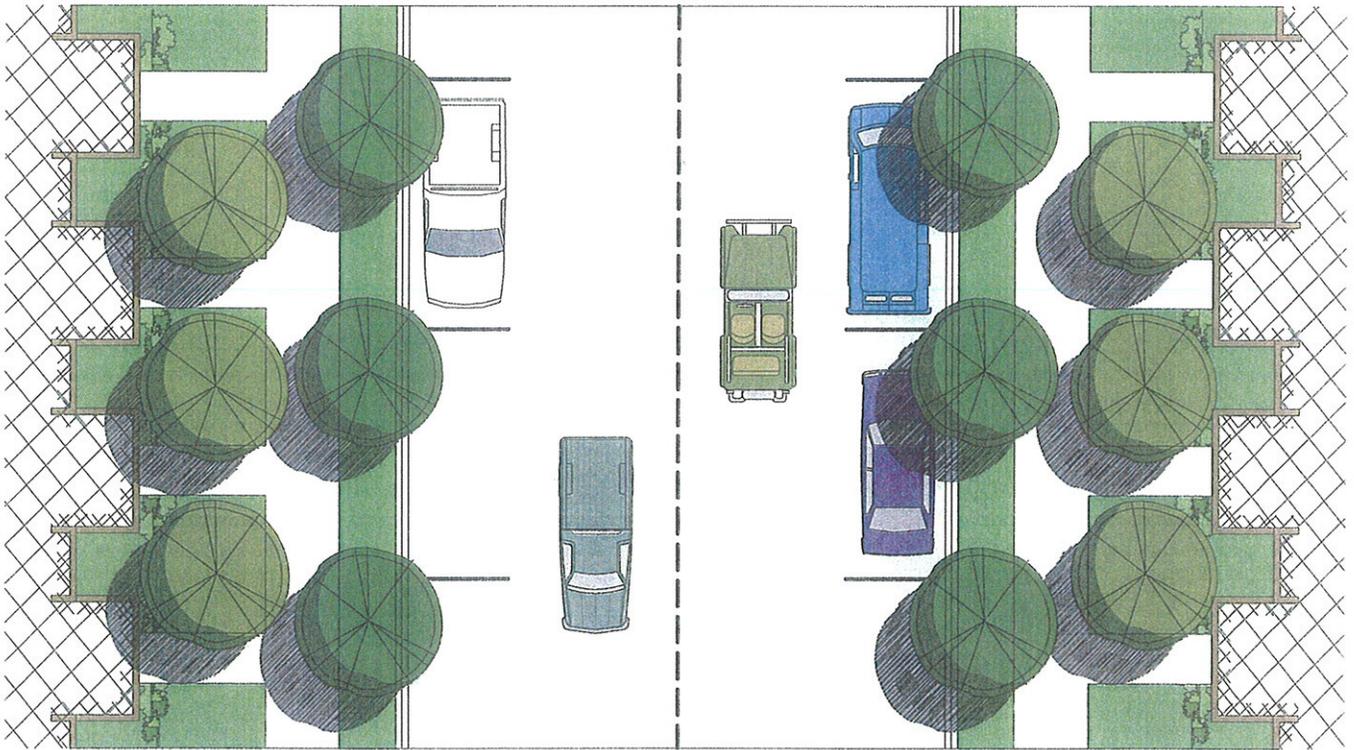
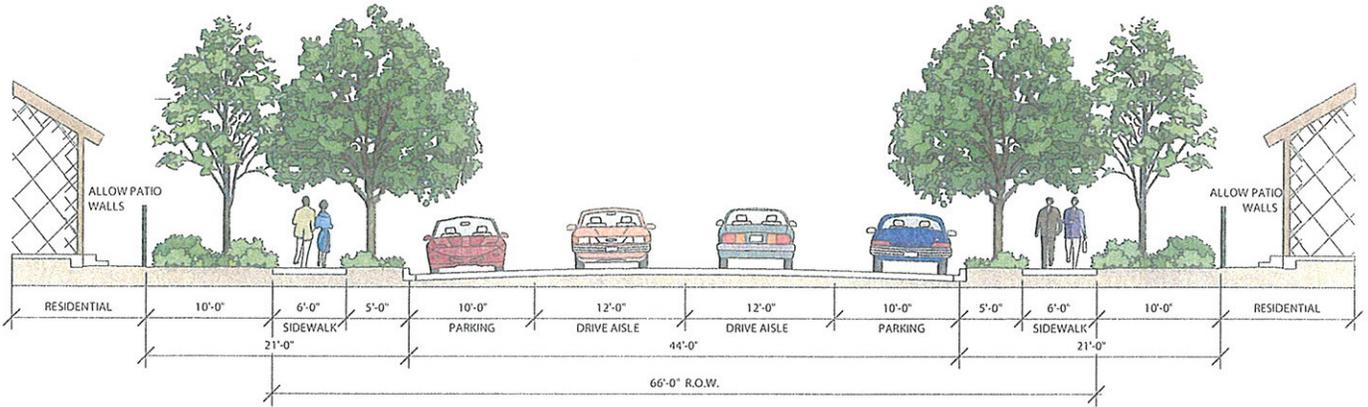
March 2006



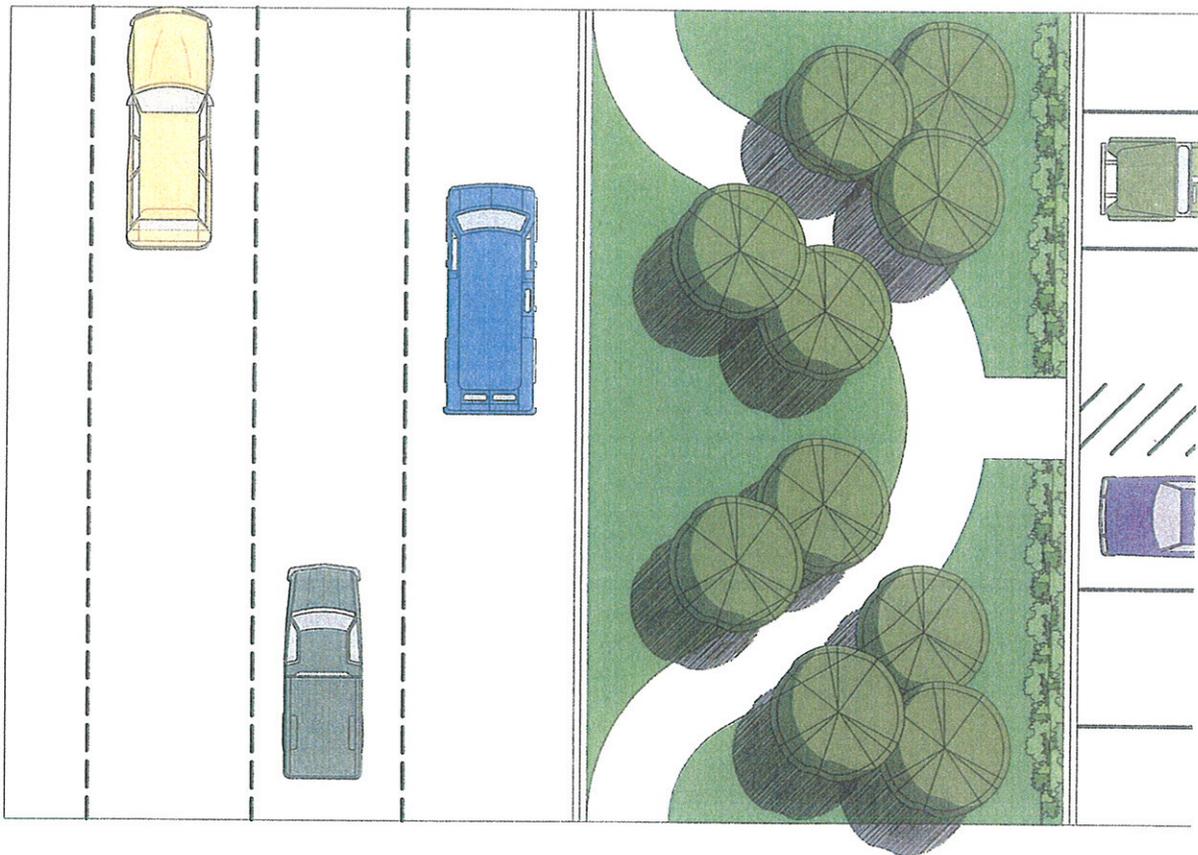
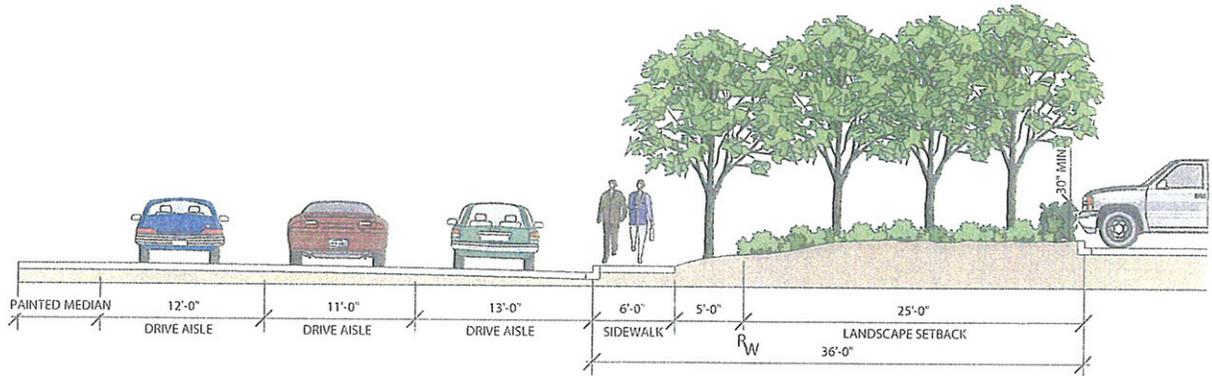
Section-C
Typical Street Section along Monroe Street
Central Highway III Corridor Specific Plan



Section-D
Typical Street Section along Rubidoux Street
Central Highway III Corridor Specific Plan



Section-F
Typical Street Section along Plaza Avenue at Existing Residential
Central Highway III Corridor Specific Plan

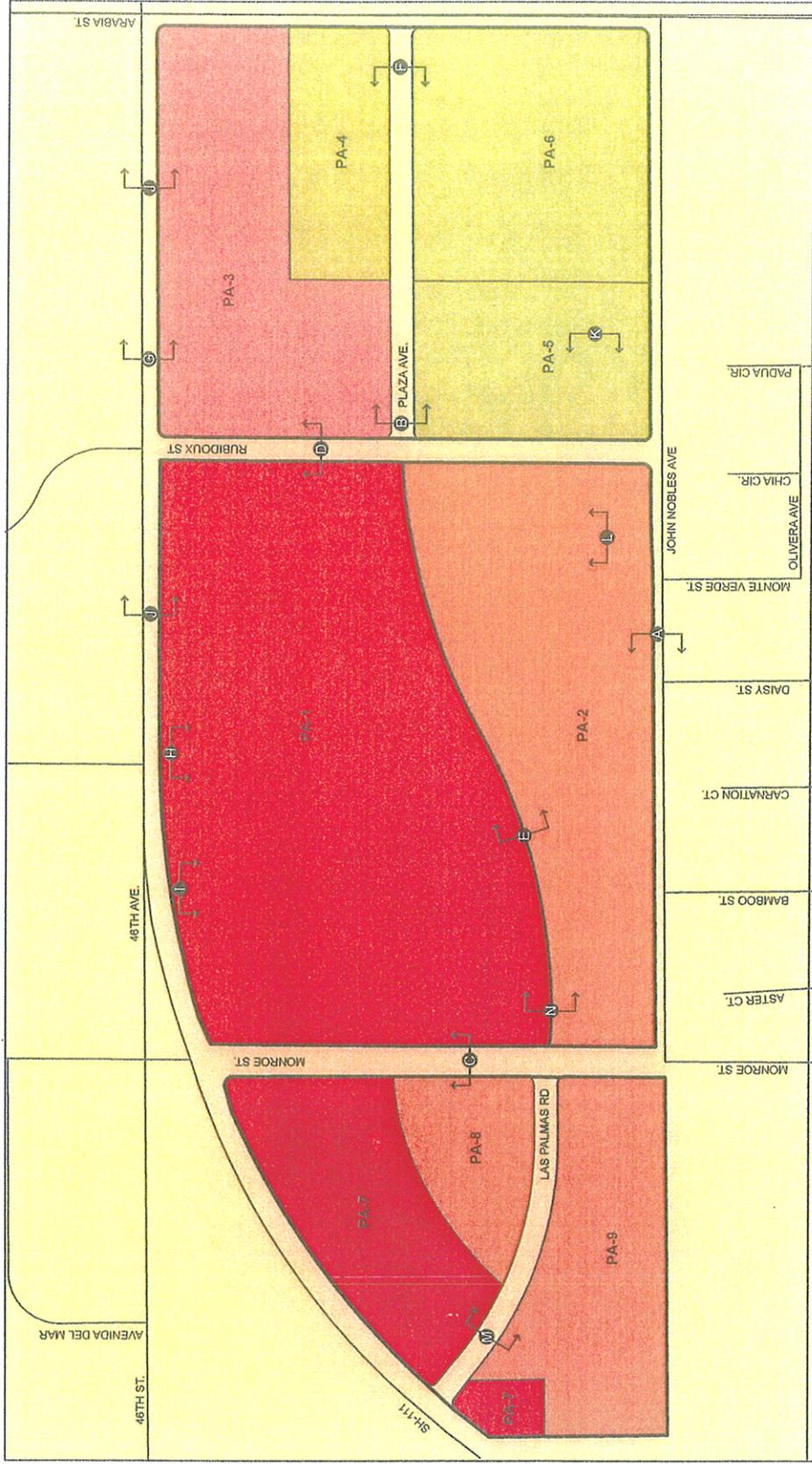


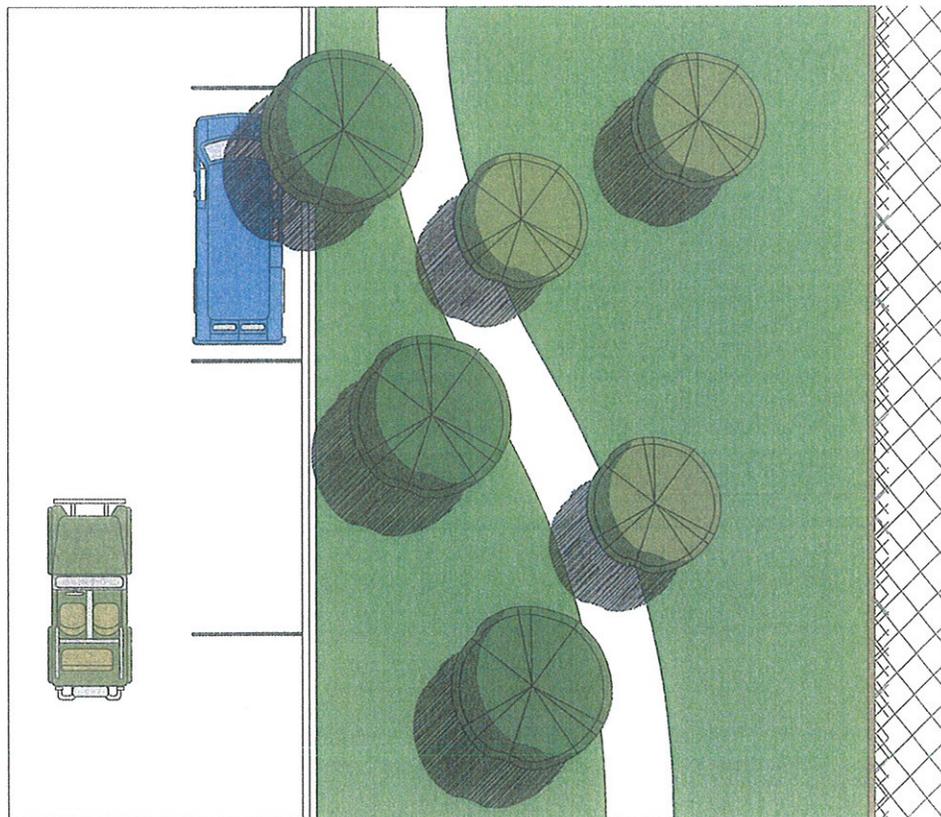
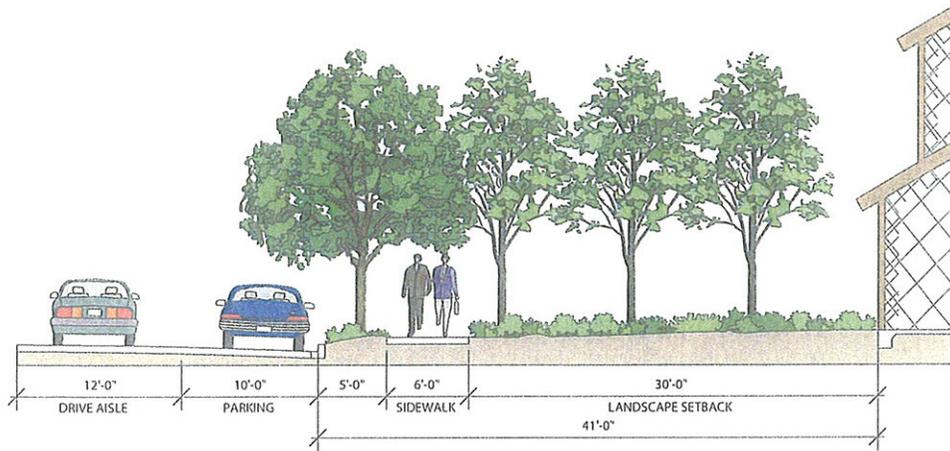
Section-G
 Typical Street Section along Highway III
 Central Highway III Corridor Specific Plan

5-3

not to scale

March 2006





Section-A
Typical Street Section along John Nobles Avenue
Central Highway 111 Corridor Specific Plan

5-3

5.9.2 Public Art

It is the desire of the City of Indio to enhance the cultural and aesthetic environment of the City. The provision of art in public places is one means to achieve this goal. New developments shall comply with the Municipal ordinance pertaining to art in public places.

The public art component of the project shall be reviewed and approved in conjunction with the review and approval of Design Review and Site Plan Review.

5.10 DRIVE-THRU FACILITIES

Non-food related land uses with drive-thru facilities require approval of a Conditional Use Permit in the Neighborhood Commercial-Specific Plan and Community Commercial-Specific Plan Zoning Districts (Restaurants with drive-thru are not permitted in the project area). In addition to the required landscape setback, drive-thru windows shall be screened from view from public rights-of-way by a combination of landscaping and lattice work, screening walls, colonnades, or comparable structures. An additional 5' of landscaped setback shall be required between the drive-thru drive aisle and the adjacent required streetside setback (See Exhibit 5-3, Section- J).

5.11 LANDSCAPING

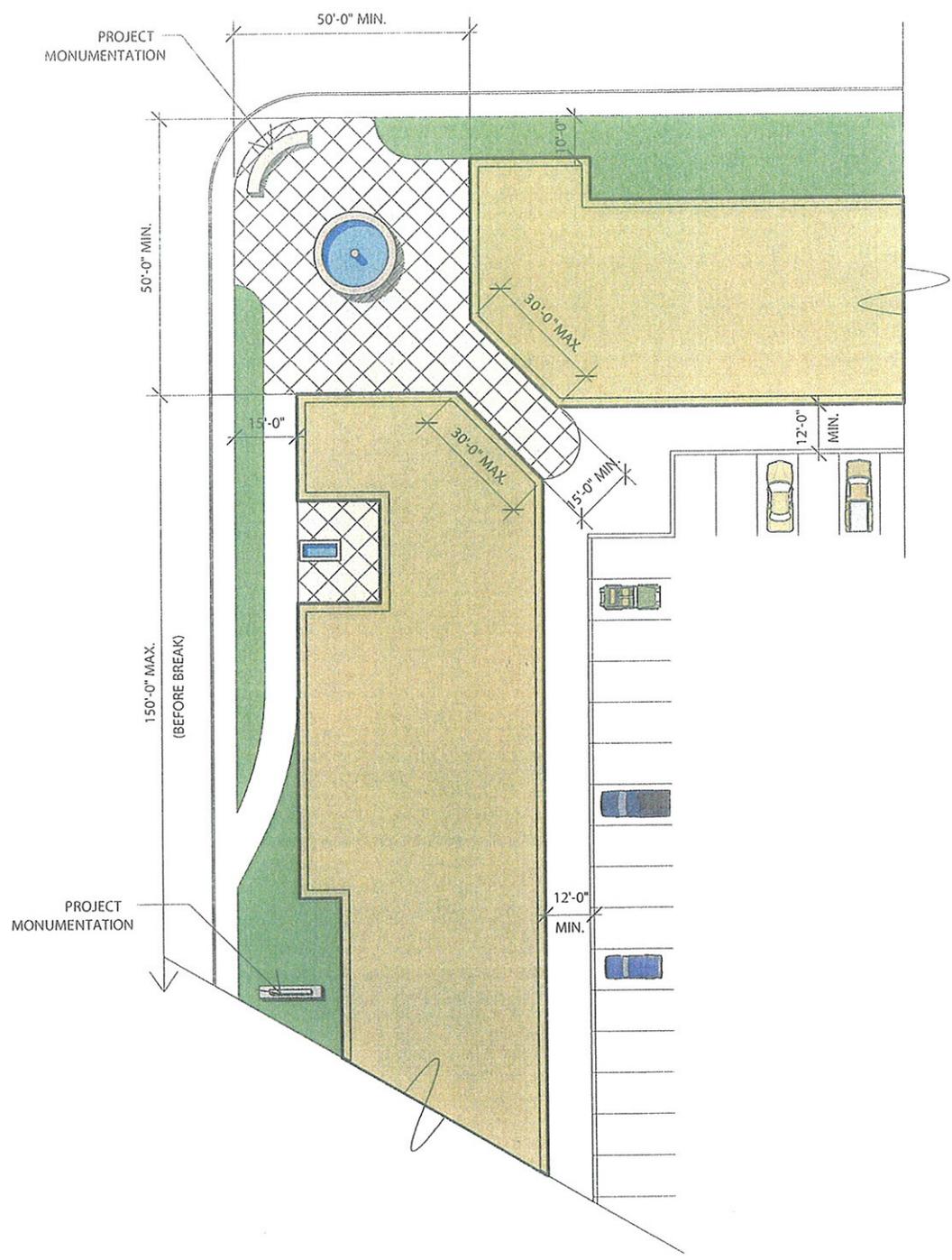
5.11.1 Rights-of-Way

All public rights-of-way between the back of curb and the nearest property line within the Specific Plan area shall be improved as part of new development on adjoining private property. Improvements shall include public sidewalks and pedestrian ways as indicated in Exhibit 5-3. Pedestrian ways may be partially located on private property. Rights-of-way landscaping shall be 5' in width and include street trees and groundcover consistent with Exhibit 5-3. Trees shall be planted at distances no greater than 25' on center in the landscaped rights-of-way areas.

5.11.2 John Nobles Avenue Landscaped Buffer

In order to create an attractive buffer between the mixed used properties on the north side of John Nobles Avenue and the single-family residential neighborhood to the south, a 30' wide landscape setback is required. The setback shall include groundcover and a minimum of two rows of trees as indicated in Exhibit 5-3, Section A.

[This page intentionally left blank]



[This page intentionally left blank]

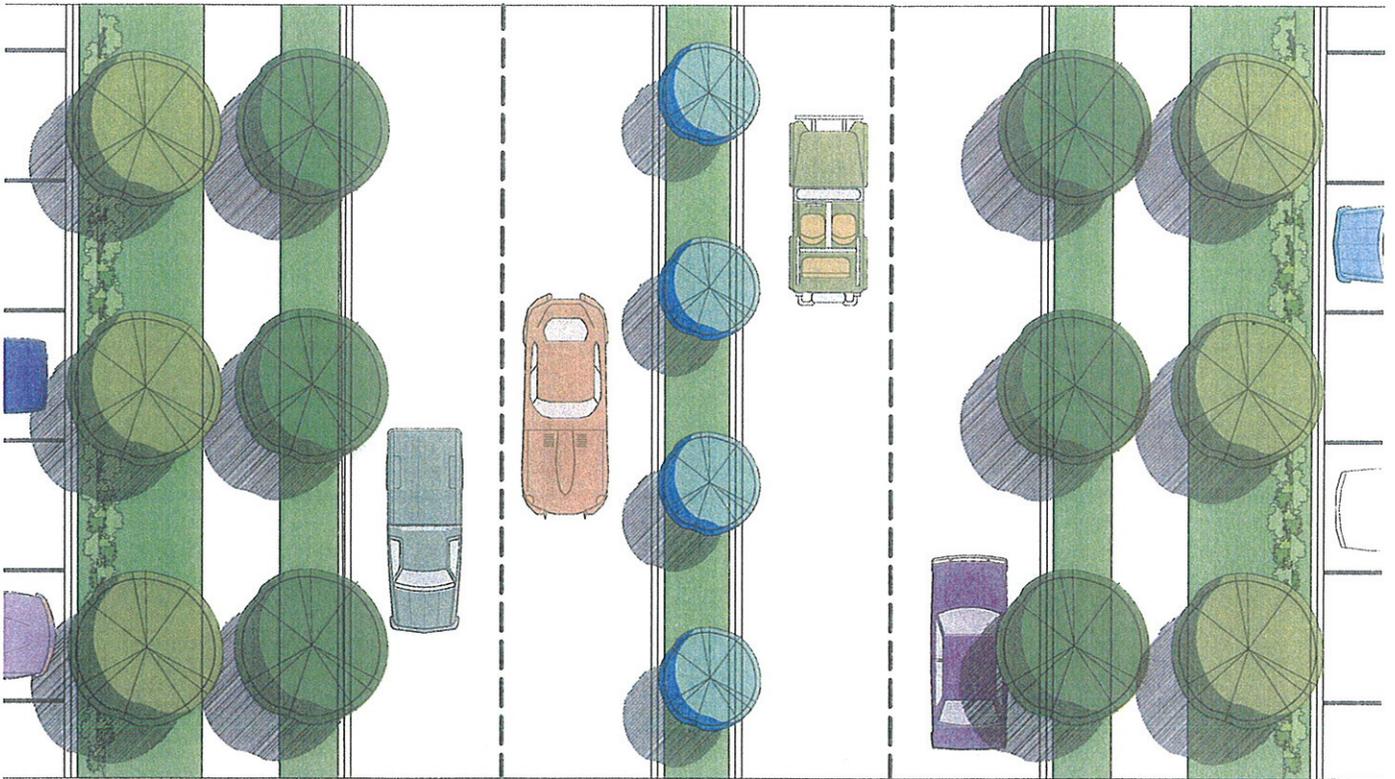
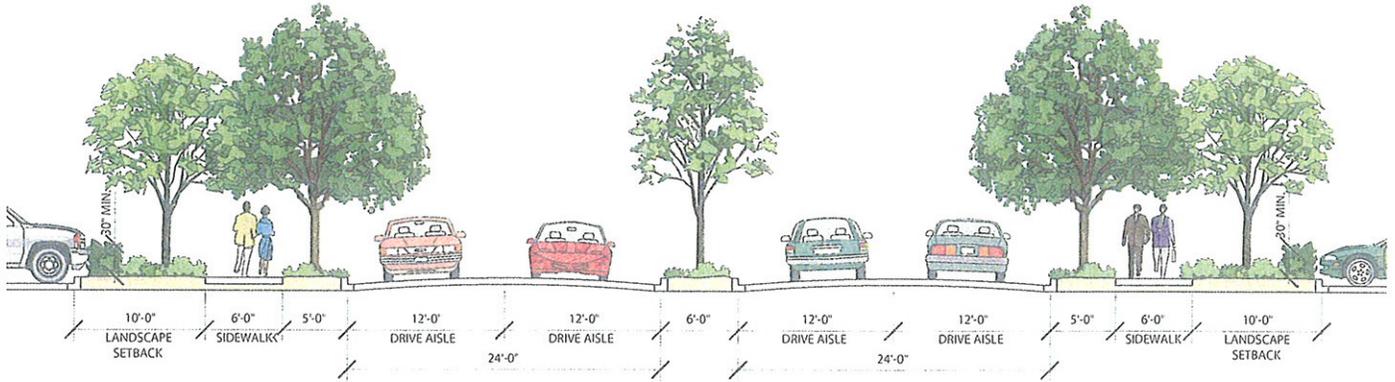
5.9 PUBLIC AMENITIES

5.9.1 Pedestrian Plazas and Corridors

Development on properties with setbacks abutting each of two public streets at the intersections of Highway 111 with Las Palmas Road, Monroe Street, Rubidoux Street and Arabia Street shall include an open plaza adjacent to the respective intersection. Plazas will enhance the appearance of the streetscape along Highway 111 and encourage pedestrian activity and travel between destination points. Plazas shall be designed and constructed consistent with the following:

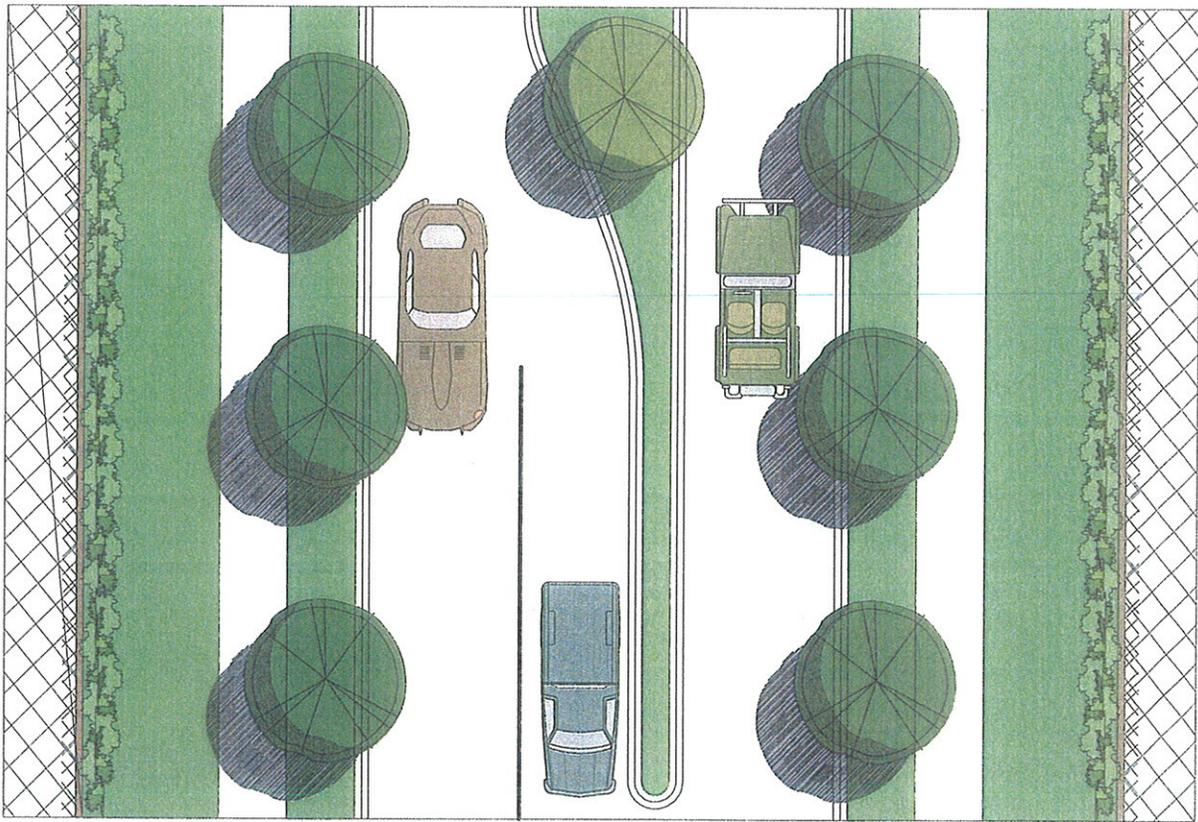
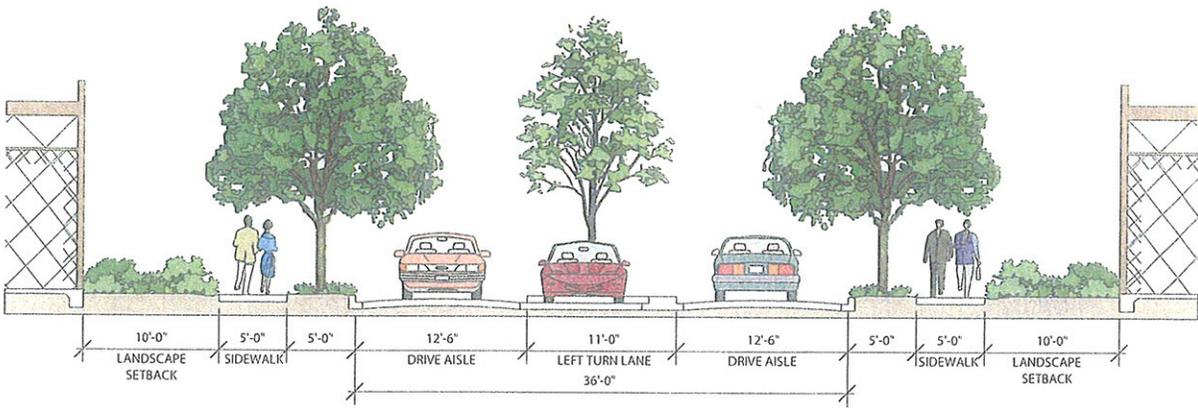
1. Minimum dimensions shall be 50' X 50'.
2. Plazas shall be surfaced with tile, pavers, enhanced pavement, or materials of comparable quality.
3. Trees, canopies, and/or awnings shall be installed to provide adequate shade.
4. Chairs, benches or seating walls shall be provided.
5. Public art shall be provided (see Section 5.9.2).

Buildings adjacent to setbacks abutting each of two public streets at the intersections of Highway 111 with Las Palmas Road, Monroe Street, Rubidoux Street and Arabia Street shall incorporate a pedestrian/view corridor at least 15 feet in width and extending from the open plaza, defined above, to the interior of the subject development. The preceding standards are illustrated in Exhibit 5-2.



Section-H
Section & Plan at Major Entry
Central Highway III Corridor Specific Plan

5-3

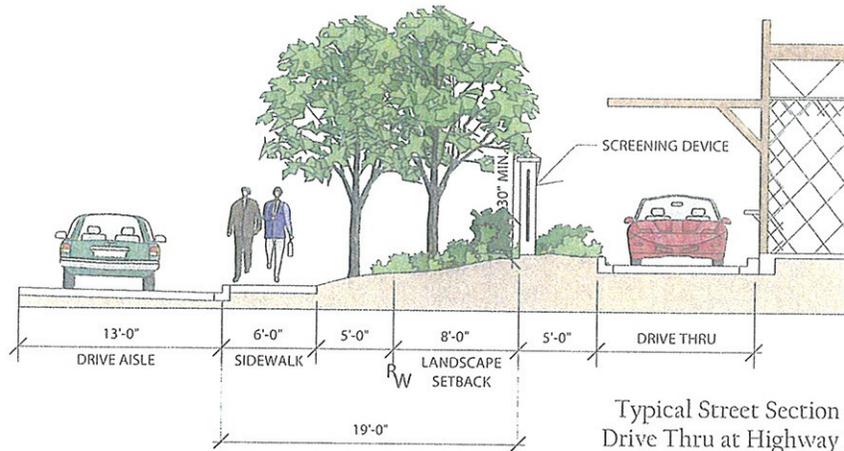


Section-I
Section & Plan at Minor Entry
Central Highway III Corridor Specific Plan

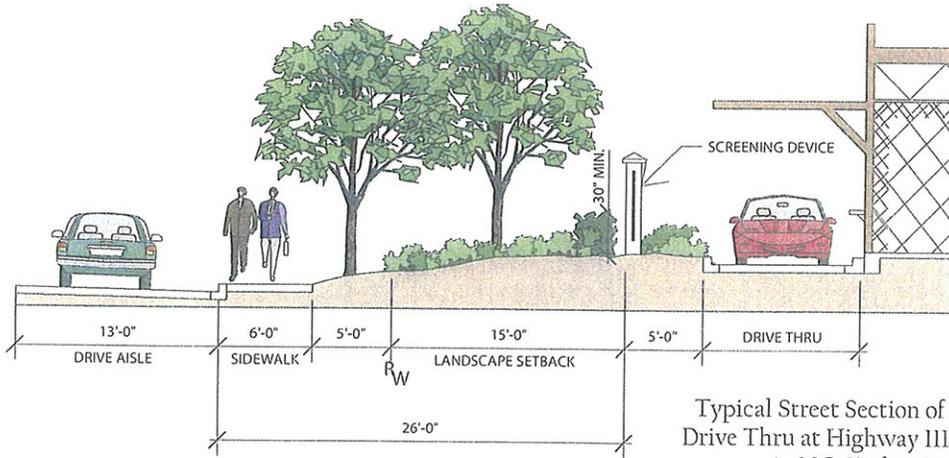
5-3

not to scale

March 2006

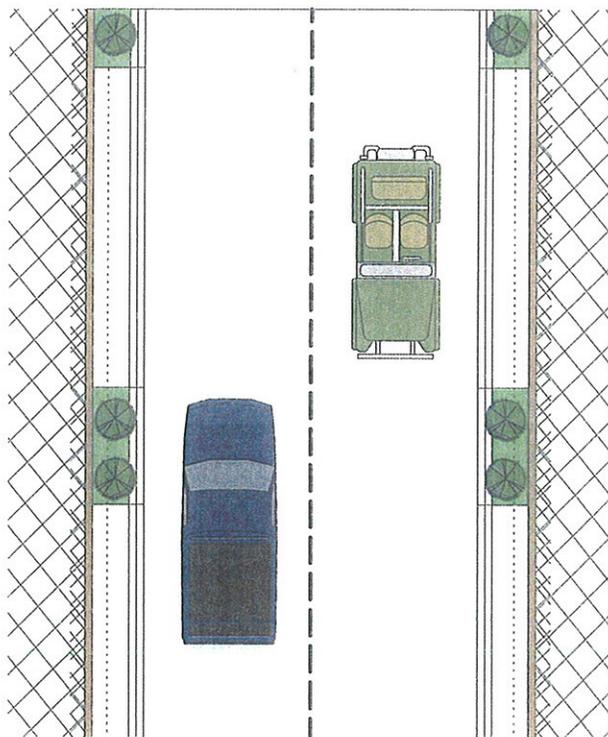
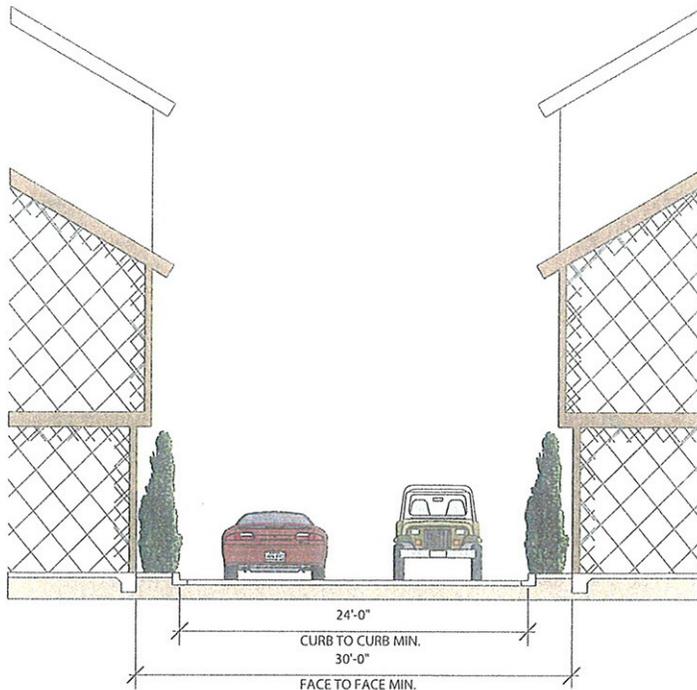


Typical Street Section of Drive Thru at Highway III in CC-SP district



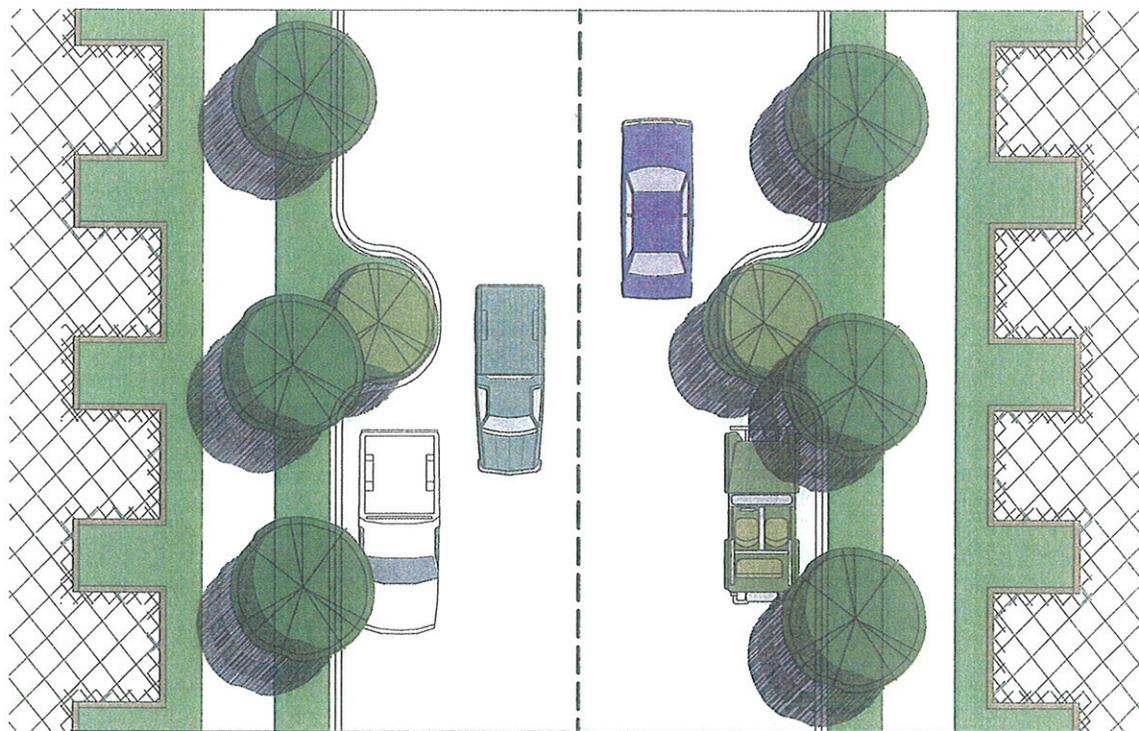
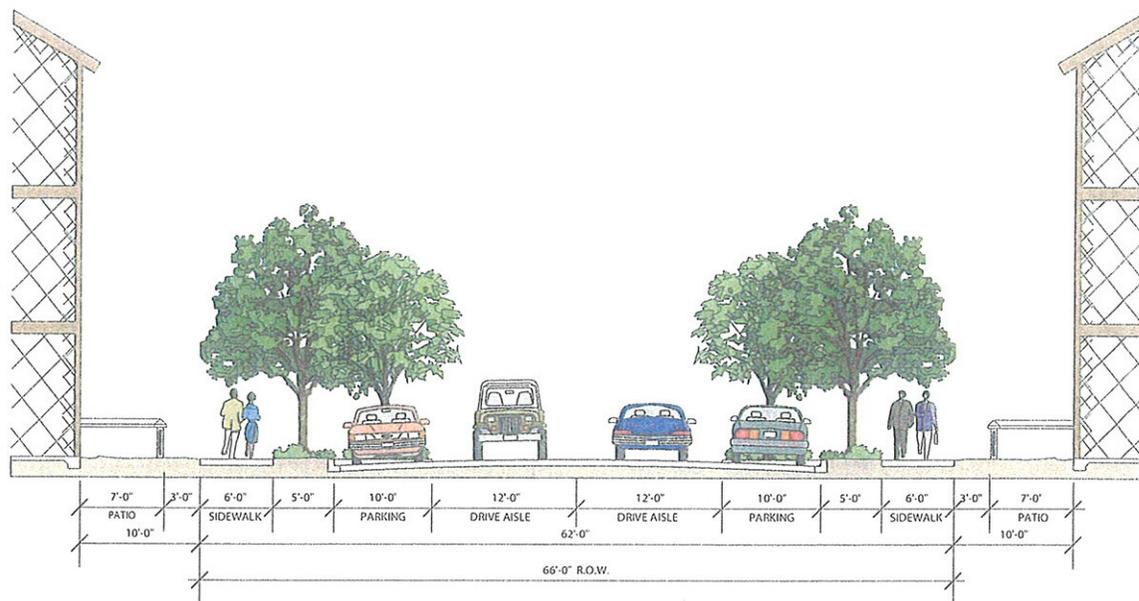
Typical Street Section of Drive Thru at Highway III in NC-SP district

Section-J
 Typical Street Section of Drive Thru
 Central Highway III Corridor Specific Plan



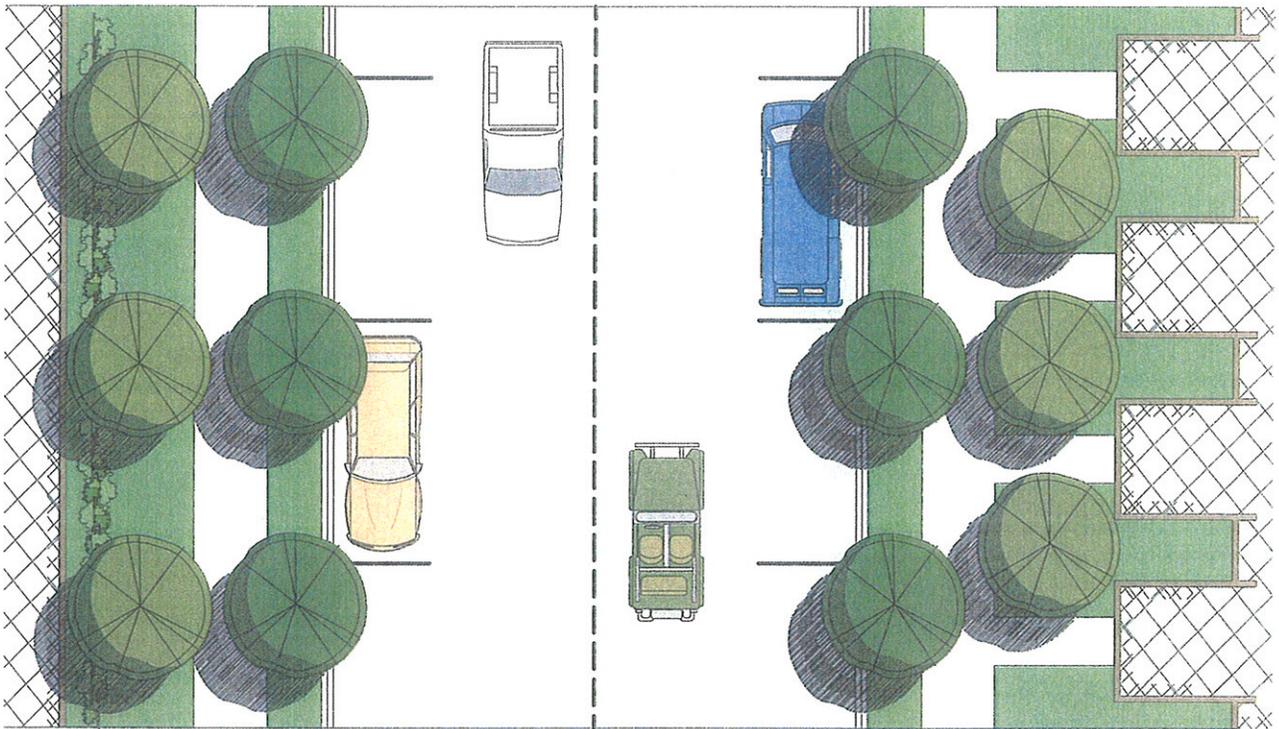
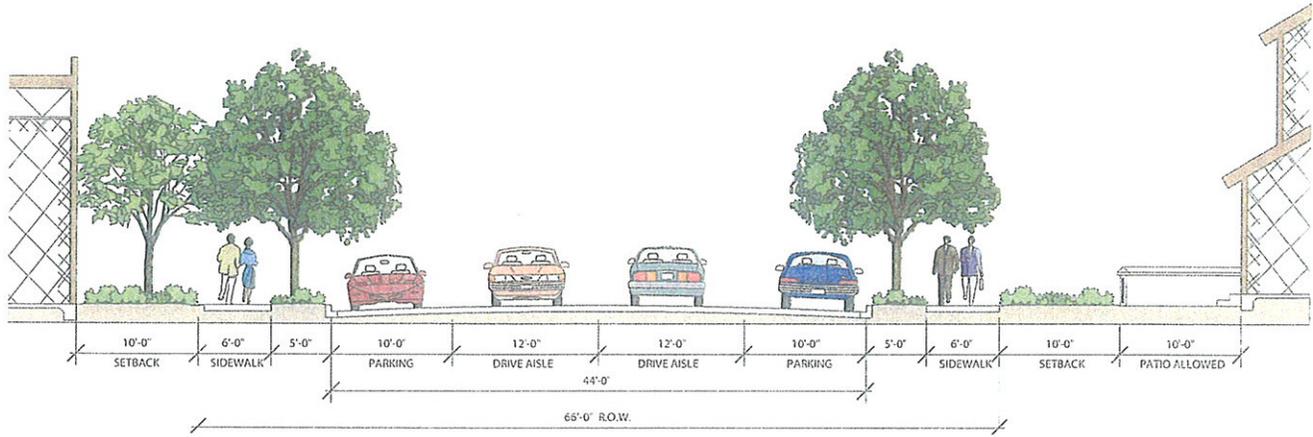
Section-K
Typical Alley Section
Central Highway III Corridor Specific Plan

5-3



Section-L
 Typical Residential Street Section
 Central Highway 111 Corridor Specific Plan

5-3

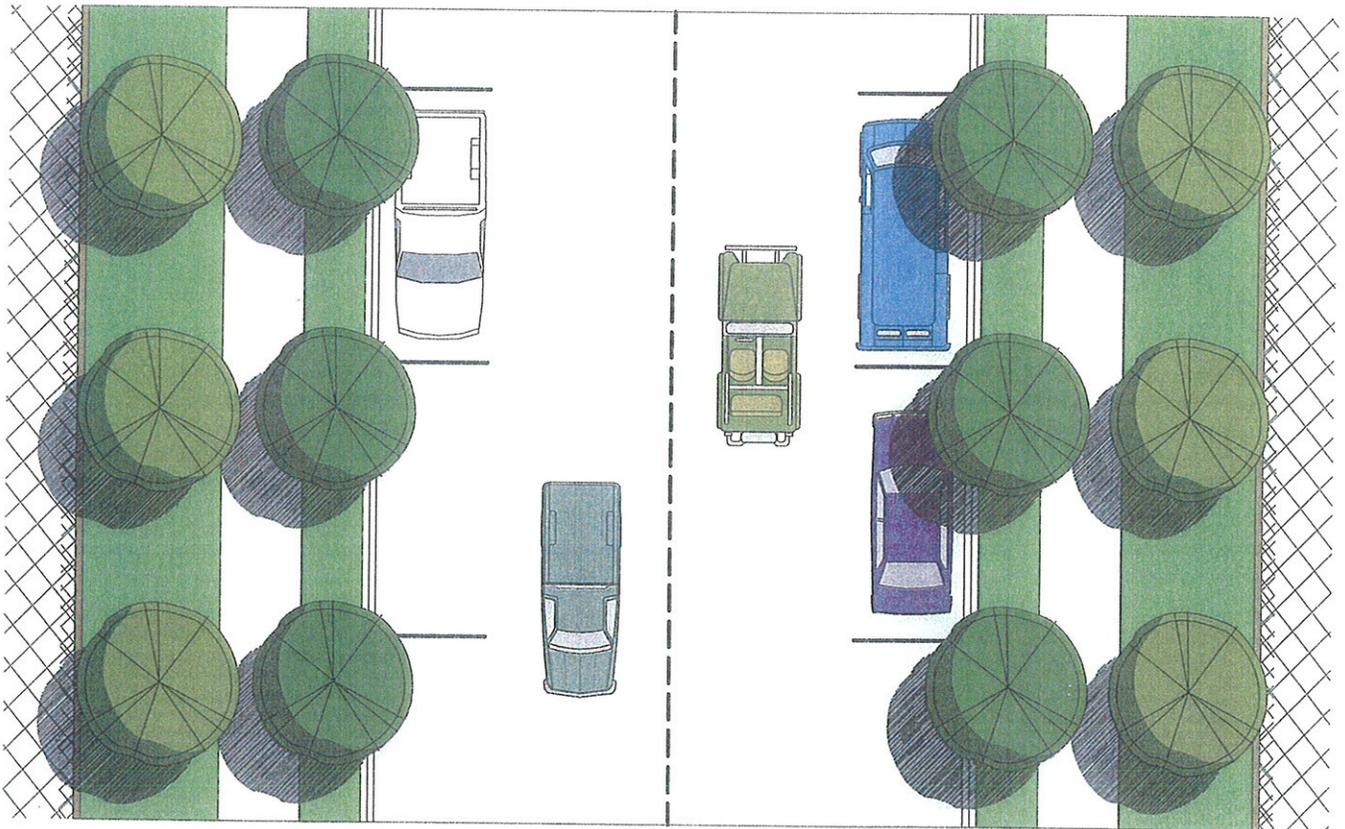
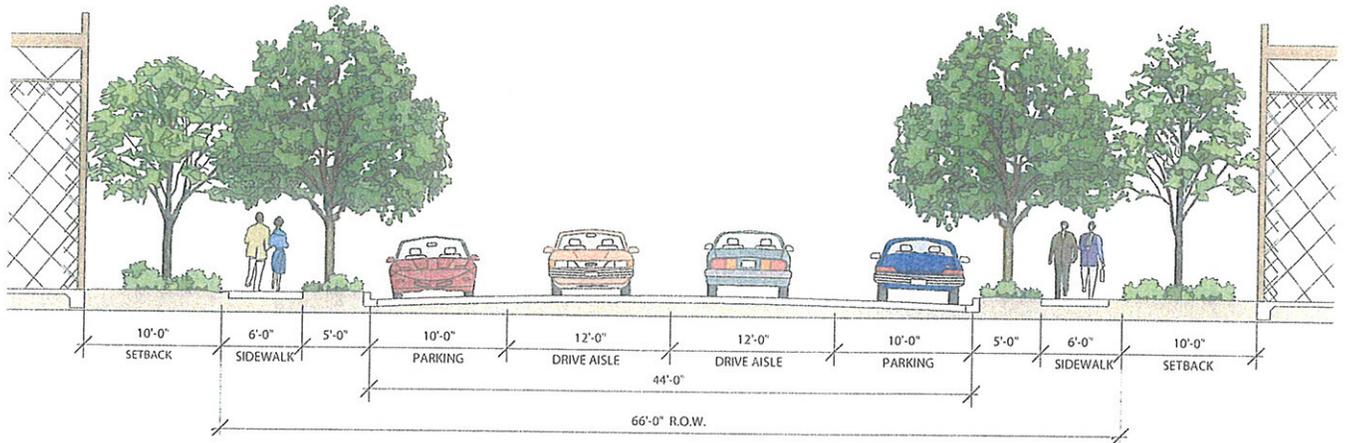


Section-M
Typical Street Section at Las Palmas Road
Central Highway III Corridor Specific Plan

5-3

not to scale

March 2006



Section-N
Typical Commercial Street Section
Central Highway III Corridor Specific Plan

5.11.3 Landscaped Setbacks

Areas defined by application of the minimum setback requirements set forth in Table 5-3 shall be landscaped consistent with this section. Landscaped setbacks shall be provided and maintained open to the sky and unobstructed by parking spaces, drive aisles, or structures except for signs as may be permitted consistent with the sign regulations in this document and driveway approaches from adjacent public streets.

In all districts, a minimum of 75% of the required setback shall be planted with groundcover, shrubs and trees. Trees shall be planted a minimum of 15' on center or clustered in groups of three. The minimum number of trees shall be calculated by dividing the total linear feet of street frontage, including driveways, by 15. The number of trees resulting from this calculation is the minimum standard. All landscaping shall be kept in an orderly condition and shall be equipped with a permanent irrigation system.

Landscaping should complement building architecture, enhance parking lots and soften hard surfaces. The tree specimens permitted in the rights-of-way and other landscaped areas within the Specific Plan are as follows:

- Shade Trees
 - Acacia farnesiana
 - Acacia tortillas
 - Acacia xanthophloea
 - Lagerstoemia indica
 - Leucaena pulverulenta
 - Olea europaea
 - Prosopis alba
 - Prosopis chilensis
 - Quercus suber
 - Ulmus pumila

- Accent Trees
 - Bauhinia variegata
 - Chorisia speciosa
 - Phoenix canariensis
 - Phoenix dactylifera
 - Washington robusta
 - Washington filifera

- Shrubs
 - Ambrosia deltoidea
 - Ambrosia dumosa
 - Anisacanthus puperulus

Caesalpinia pulcherrima
Carissa macrocarpa
Hyptis emoryi
Plumbago scandens
Rosemarinus officinalis

The plant palette was selected because the specimens are native to warm deserts and because of their low-water requirements. Substitution of the list must be approved by the Director of Community Development.

5.12 VEHICLE ACCESS, OFF-STREET PARKING, AND LOADING

5.12.1 Purpose

The intent and purpose of these regulations is to provide properly designed parking areas and loading berths adequate in capacity, location and design to prevent traffic congestion and a shortage of curb spaces. Off-street parking facilities shall be provided incidental to new uses and alterations and enlargements of existing uses. The number of parking spaces shall be in proportion to the need created by the particular use. Off-street parking and loading areas are to be established in a manner that will ensure their usefulness, protect public safety, and where appropriate, buffer surrounding land uses from their impact.

5.12.2 Application

Off-street parking shall be provided for any new building constructed and for any new use established; for any addition or enlargement of an existing building or use; and for any change in the occupancy of any building or the manner in which any use is conducted that would result in additional parking spaces being required, unless an equivalent substitute number of such spaces is provided and maintained conforming to the requirements of this chapter except as otherwise provided in this chapter.

5.12.3 Vehicle Access

Vehicular entries to sites within the Specific Plan area shall be designed and constructed consistent with the standard specifications of the City of Indio Engineering Department and the requirements in this section.

Vehicular entries into commercial or mixed use sites shall be separated by a distance of 300 feet except that one vehicular entry shall be permitted to each legal lot with street frontage existing as of the date of adoption of the Specific Plan. Vehicular entries shall be either Major or Minor.

Major entries are comprised of two inbound vehicle lanes and two outbound lanes and include a minimum 6' wide landscaped center median. Landscaping and pedestrian walkways shall be located between the drive aisle and any building/parking area in accordance with Figure 5-3, Section H.

Minor entries are comprised of one inbound vehicle lane and two outbound vehicle lanes and include a landscaped center median. Landscaping and pedestrian walkways shall be located between the drive aisle and any building/parking area in accordance with Figure 5-3, Section I.

It is the intent of the Specific Plan to ultimately eliminate non-conforming vehicular entries and reduce the number of curb-cuts on public rights-of-way. Plans for all proposed driveways shall be subject to review and approval by the City Engineer.

5.12.4 Change in Use or Intensity

When the use of any premises is changed, enlarged, altered, expanded or intensified such that additional parking is required, the additional parking to meet the requirements of this chapter shall be provided for the changed, enlarged, altered, expanded or intensified portion of the occupancy or use, except as otherwise provided in this chapter.

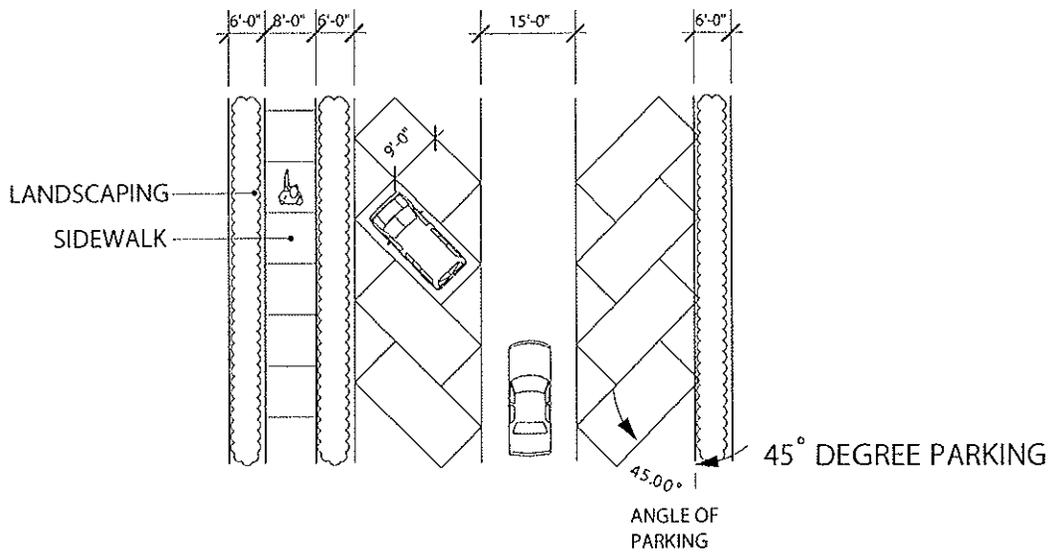
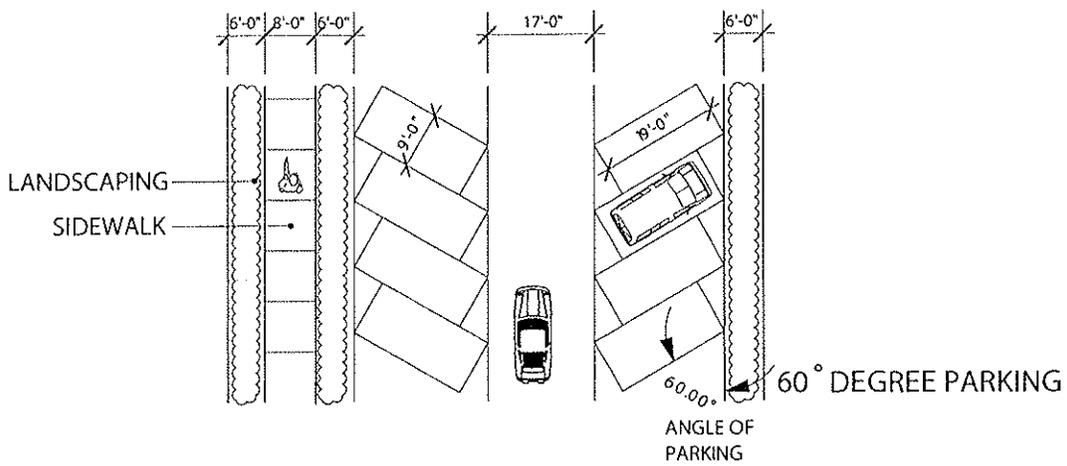
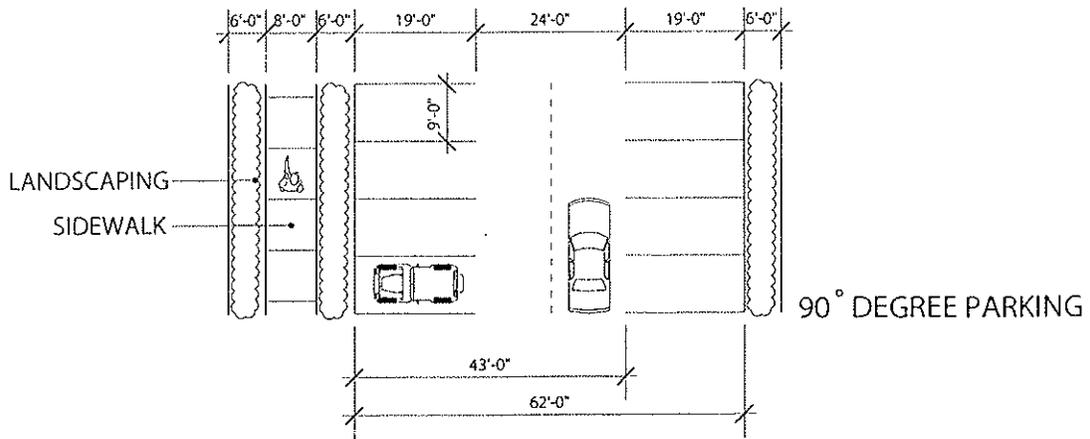
5.12.5 General Development Standards

- a) Location of Parking Spaces. The required parking spaces or garages shall be located on the same building site except as otherwise provided by this title. Property within the ultimate right-of-way of a street or highway shall not be used to provide required parking or loading facilities.
- b) Design and Maintenance. All off-street parking spaces and areas required by this chapter shall be designed and maintained to be fully usable for the duration of the use requiring such areas and spaces.
- c) Tandem Spaces. Tandem spaces shall only be permitted for those spaces which exceed the requirements of this title.
- d) Stall Dimensions. Each off-street parking stall shall consist of a rectangular area not less than nine (9) feet wide by nineteen (19) feet long, except as otherwise provided in this chapter. End spaces where clear back-out space is restricted on one side shall be 11 feet in width.
- e) Aisle Widths. The minimum width for a parking lot aisle for one-way traffic shall be 15 feet. Aisle widths adjusted for angle of parking spaces shall be as follows:

Table 5-4: Parking Aisle Widths

Parking Angle	Aisle Width
Less than 60 degrees	15'
60 degrees	17'
90 degrees	24'

These standards are illustrated in Exhibit 5-4.



f) Landscaping Requirements.

1. A minimum of 15% of the total off-street open parking area shall be landscaped with a mixture of trees, shrubs, groundcover, other plant material and hardscape material. A minimum of 1/3 of the required landscaping shall be distributed within the interior of the parking facility and the remaining 2/3 of the required landscaping shall be provided as peripheral planting on the exterior edges of the parking area. All landscape areas shall be well maintained in perpetuity.
2. A minimum of one 24-inch box tree shall be required for each four parking spaces. Said trees are intended to provide shade to parked vehicles and shall be of a type adapted for the climate of the Coachella Valley. Street side planters may contain palm trees at a rate of not more than 25%.
3. Irrigation systems shall be installed within parking lot landscaping. The irrigation system shall be an automatic system with an irrigation timer and two drip or bubbler heads per tree to provide adequate deep root irrigation.
4. All parking lot landscaping shall be maintained in perpetuity, with all maintenance to include, but not limited to, irrigation corrections, fertilization, pruning and staking. All maintenance shall be performed with the overall goal of providing maximum plant health with minimum damage to hardscape, vehicles and persons.
5. Landscape and automatic system plans, including the type and location of plant materials, shall be submitted to and approved through the design review process either as a part of a review process applicable to the subject property or prior to the issuance of building permits if no such review process is required.
6. Parking Lot Hardscape Requirements. Parking lot dividers, islands, planters and planting areas shall be a minimum of four feet wide and four feet long. Trees are to be planted a minimum of three feet from walls, walks or buildings. To protect hardscape and encourage deep root development, root barriers shall be installed during planting operations.
7. Screening Requirements. All off-street parking areas shall be screened to minimize the visual impact on adjacent streets and properties. Any open areas in the interiors so formed shall be landscaped with appropriate plant materials.
8. Perimeter Guard Requirements. Bumper guards or wheel stops shall be provided for all parking spaces abutting the perimeter of a parking

area where such perimeter is abutting a building, structure, wall, public right-of-way or lot line, except spaces within a garage or carport.

9. Lighting Requirements. The equivalent of one foot candle of illumination shall be provided throughout the parking area. All lighting shall be on a time-clock or photo-sensor system. Lighting used to illuminate parking areas shall be designed and located to confine direct rays to the premises. No spillover or bleeding of light beyond the property line or into the sky shall be permitted. Light standards shall be a maximum of 30 feet in height in nonresidential areas and twenty feet in height in residential areas. Illumination shall not include low pressure sodium or similar lighting sources.
- g) Marking Requirements. All open parking stalls shall be clearly outlined with lines which are a minimum of four inches in width on the surface of the parking facility. All stalls shall be double striped with the stall widths measured from the midpoint of the double stripe markings.
- h) Paving Requirements. All areas within the parking area not used for parking stalls or maneuvering areas shall be landscaped. All areas used for the movement, parking, loading, repair or storage of vehicles shall be paved with either:
 1. Concrete, to a minimum thickness of 3.5 inches; or
 2. Asphalt pavement, to a minimum thickness of 1.5 inches over 4 inches of crushed rock, gravel or similar material; or
 3. Other surfacing material providing equivalent life, service and appearance in the opinion of the Community Development Director. Concrete approaches shall be provided for ingress and egress.
- i) Drainage Requirements. All areas used for the movement, parking, loading, repair or storage of vehicles shall be graded and drained to dispose of all surface water. Drainage shall not be permitted across the surface of sidewalks or driveways.
- j) Maneuvering Requirements. The off-street parking area shall be designed so that a vehicle within a parking area will not be required to enter a street to move from one location to another within the parking area.
- k) Use Restriction. The required parking area shall not be used for any use other than the temporary storage of operational motor vehicles during the time that the use requiring the parking is in operation.

- l) Valet Parking. Uses utilizing valet parking must have an approved valet parking plan which includes insurance provisions for patrons. Such provisions must be in a form acceptable to the City Attorney.
- m) Joint Use Parking. Dual use of parking may be allowed where the operating hours of the uses involved do not overlap. Joint use of parking shall not exceed sixty percent of the required spaces of any uses involved. The agreement for the joint use shall be in the form of a recorded covenant and shall be in a form acceptable to the City Attorney. The approval for the joint use can be granted only by the Planning Commission.
- n) Parking for the Handicapped. "Handicapped" spaces shall be provided in accordance with state and federal regulations and shall be considered in the calculation of required spaces.
- o) Pedestrian Walkways. Walkways shall be provided within parking areas and configured so that pedestrians moving from parking spaces to buildings are separated from drive aisles to the maximum extent feasible. Walkways shall be a minimum of 8' in width as illustrated in Exhibit 5-5.

5.12.6 Required Parking

Required parking for each permitted use is identified in Table 5-5 "Required Parking". The required parking shall be the total of that for all uses on the site. In calculating the square footage to be used in determining required parking, areas for restrooms, elevator shafts and utility rooms shall be exempted. The requirements for loading spaces are identified in Table 5-6, Required Loading.

Table 5-5: Required Parking

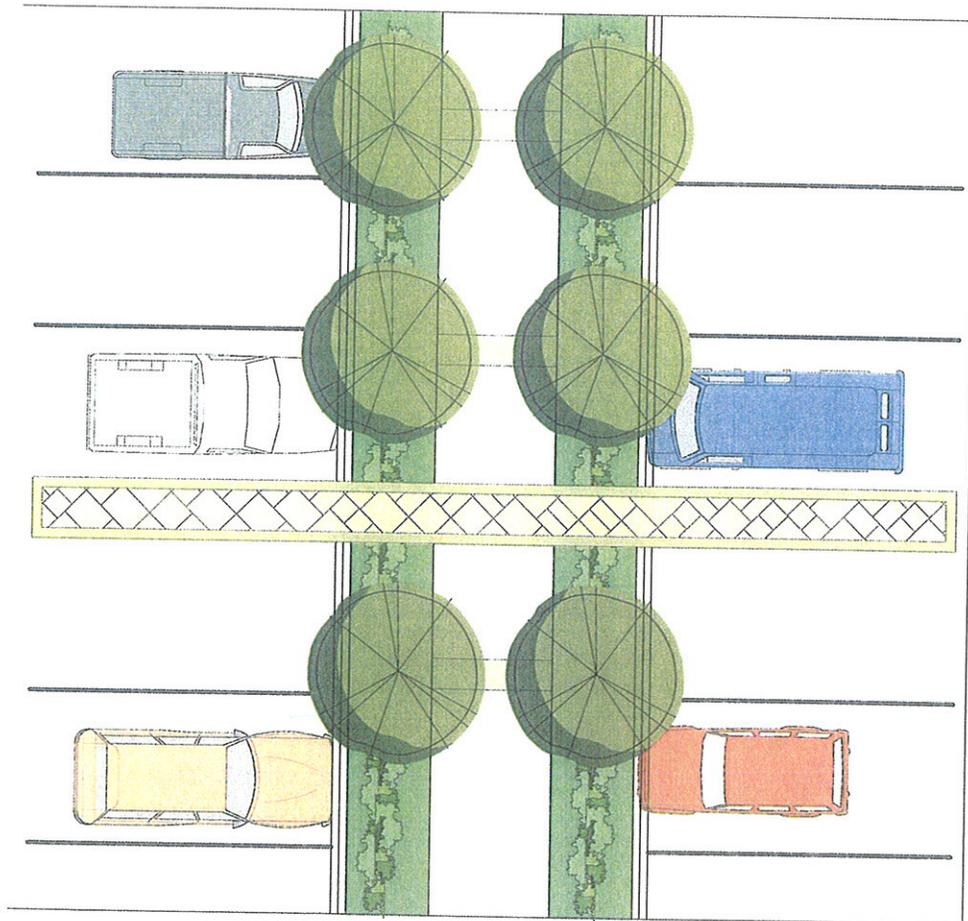
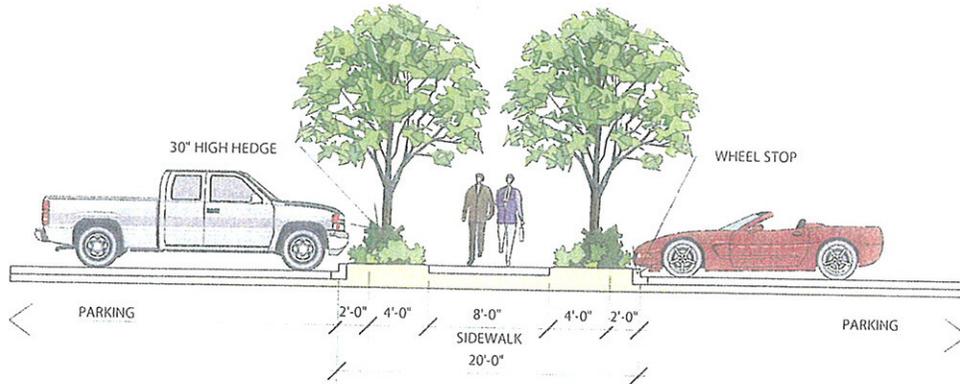
USE	REQUIRED NUMBER OF STALLS
RESIDENTIAL	
Multi-family units	Stalls per Unit: Garage Other Visitor
1 bedroom or less	1.0 0.5 .2
2 bedrooms or more	1.0 1.0 .2
	*Fifty percent of all spaces not in a garage shall be in a covered carport
Senior Housing	One space per each two beds or one space per each residential unit, plus 1 stall for the resident manager
Single-family units	2 stalls within a garage, plus .2 stalls per dwelling unit
COMMUNITY	
Community Center	One space for each 400 square feet of floor area
Public Library	One space for each 400 square feet of floor area
Social Club	One space for each 50 square feet
OFFICE	
Business and Professional office	Four (4) spaces for each 1,000 square feet of floor area
Financial Institution	Five (5) spaces for each 1,000 square feet of floor area

Veterinary Clinic	Four (4) spaces for each 1,000 square feet of floor area
COMMERCIAL – RETAIL	
Bed and Breakfast	1.1 spaces for each guest room
Day Care Center	Five (5) spaces for each 1,000 square feet of floor area, plus two loading spaces
Furniture Store	One space for each 400 square feet of floor area
General Retail	Four (4) spaces for each 1,000 square feet of floor area
Health Club	Ten (10) spaces for each 1,000 square feet
Hotel	1.1 spaces for each guestroom (ancillary uses calculated separately)
Personal Service	Four (4) spaces for each 1,000 square feet of floor area
Restaurants, cafes, bars, lounges and similar uses serving food and drink	Ten (10) spaces for each 1,000 square feet of floor area (a minimum of ten (10) spaces are required)
Theater	One space for each two seats

5.12.7 Off-Street Loading Requirements

Every commercial building hereafter erected or established, shall have and maintain loading spaces as provided subject to the following standards:

- a) Large truck loading spaces shall be 12 feet wide by 45 feet long, exclusive of aisle maneuvering space, and shall have a vertical clearance of 14 feet. Reduced truck loading spaces shall be 12 feet wide by 25 feet long and shall have a vertical clearance of 12 feet.
- b) Sufficient room for turning and maneuvering vehicles shall be provided on the site so that vehicles shall cross a property line only by driving forward.
- c) Each loading berth shall be accessible from a street or alley or from an aisle or drive connecting with a street or alley.
- d) Entrance from and exits to streets and alleys shall be designed to minimize traffic congestion.
- e) The loading area, aisles, and access drives shall be paved so as to provide a durable, dustless surface and shall be so graded and drained so as to dispose of surface water without damage to private or public property, streets or alleys.



- f) Bumper rails shall be provided at locations where needed for safety or to protect property.
- g) No repair work or servicing of vehicles shall be conducted in a loading area.
- h) Off-street loading facilities shall be located on the same site with the use for which the berths are required.
- i) If more than one use is located on a site, the number of loading berths provided shall be equal to the sum of the requirements prescribed in this title for each use. If more than one use is located on a site and the gross floor area of each use is less than the minimum for which loading berths are required but the aggregate gross floor area is greater than the minimum for which loading berths are required, off-street loading berths shall be provided as if the aggregate gross floor area were used for the use requiring the greatest number of loading berths.
- j) Off-street loading areas for a single use shall be considered as providing required off-street loading facilities for any other use as long as sufficient spaces are provided to meet the requirements of all uses.
- k) At the time of initial occupancy, major alterations or enlargement of a site, or of completion of construction of a structure or of a major alteration or enlargement of a structure, there shall be provided off-street loading berth requirements. The number of loading berths provided for a major alteration or enlargement of a site or structure shall be in addition to the number existing prior to the alteration or enlargement.
- l) Space allocated to any off-street loading berth shall not be used to satisfy the space requirements for any off-street parking facility.
- m) Loading space being maintained in connection with any existing main building shall thereafter be maintained so long as the building remains, unless an equivalent number of such spaces are provided on a contiguous lot in conformity with the requirements of this chapter; provided, however, that this regulation shall not require the maintenance of such space for any type of main building other than those specified above.
- n) No loading space which is provided for the purpose of complying with the provisions of this title shall hereinafter be eliminated, reduced, or converted in any manner below the requirements established in this title, unless equivalent facilities are provided elsewhere, conforming to this title.

- o) The following off-street loading spaces shall be provided for commercial uses:

Table 5-6: Required Loading Spaces

Commercial Square Footage	Required Loading Spaces	Type
Less than 6,000	0	N/A
6,000 to 15,000	1	Reduced
15,001 to 40,000	2	1 Reduced 1 Large
40,001 and over	3	2 Reduced 1 Large
Hotels	1	1 Large

5.13 SIGN REGULATIONS

5.13.1 Commercial

The following sign standards are applicable to all commercial projects within the Central Highway 111 Corridor Specific Plan area:

5.13.1.1 Sign Program

Each new commercial project or remodel on parcels ½ acre or larger shall be required to submit a sign program to ensure coordinated signage, subject to review and approval by the Community Development Director as described and set forth in Chapter 159.720 of the Indio Zoning Regulations.

5.13.1.2 Major Entry Identification Signs

Two (2) identification signs are permitted at each Major Entry subject to the following standards. Major and Minor Entries are described in Section 5.12, Vehicle Access, Off-Street Parking, and Loading. Signs on sites with more than one entry shall be a minimum of 300' apart.

- a) Sign Type: Single-faced monument.
- b) Location: One sign on each side of the entry, facing the entry intersection, setback a minimum of 10' from sidewalk and/or property lines (See Exhibit 5-6).
- c) Maximum Sign Face Area: 60 square feet.
- d) Maximum height: 6 feet.
- e) Sign Copy: Only tenants with a gross leasehold area greater than 10,000 square feet shall be identified and a maximum of six (6) tenant listings per sign face are permitted.

- f) Illumination: Illumination shall be from an external source, not internally illuminated.

5.13.1.3 Minor Entry Identification Signs

One (1) project identification sign is permitted at each Minor Entry subject to the following standards. Signs on sites with more than one entry shall be a minimum of 300' apart.

- a) Sign Type: Double-faced monument.
- b) Location: Setback a minimum of a 10' from sidewalk and/or property lines (See Exhibit 5-6).
- c) Maximum Sign Face Area: 40 square feet.
- d) Maximum height: 5 feet.
- e) Sign Copy: A maximum of six (6) tenant listings per sign face are permitted.
- f) Illumination: Illumination shall be from an external source, not internally illuminated.

5.13.1.4 Corner Property Project Identification Signs

In addition to project entry signs, one (1) project identification sign located at the corner of sites abutting each of two public streets is permitted subject to the following standards:

- a) Sign Type: Single-faced monument.
- b) Location: Setback a minimum of 5' from sidewalk and/or property lines.
- c) Maximum Sign Face Area: 40 square feet.
- d) Maximum height: 4 feet.
- e) Sign Copy: Project identification only; no tenant listings are permitted.
- f) Illumination: Illumination shall be from an external source, not internally illuminated.

5.13.1.5 Wall Signs

Each tenant is allowed primary building fascia signage on each elevation oriented toward a street, with additional blade, window, awning and canopy signs in accordance with an approved sign program.

- a) Sign Type: Individual channel letter wall signs; internally illuminated cabinet signs or painted signs are prohibited.
- b) Location: Wall signs shall be oriented away from residential properties to the maximum extent feasible.
- c) Maximum Sign Face Area: each applicable tenant frontage shall be permitted one (1) square foot of sign area for each linear foot of building frontage to a maximum of 200 square feet.

5.13.1.6 Directional/Informational/Regulatory Signs

Each project shall be allowed single or double-faced signs oriented as necessary to direct vehicle or pedestrian flow subject to the following standards:

- a) Sign Type: Signs may be freestanding or wall-mounted.
- b) Location: Interior to the site along pedestrian and vehicular routes.
- c) Maximum Sign Face Area: 6 square feet.
- d) Illumination: Signs may utilize direct or indirect illumination.

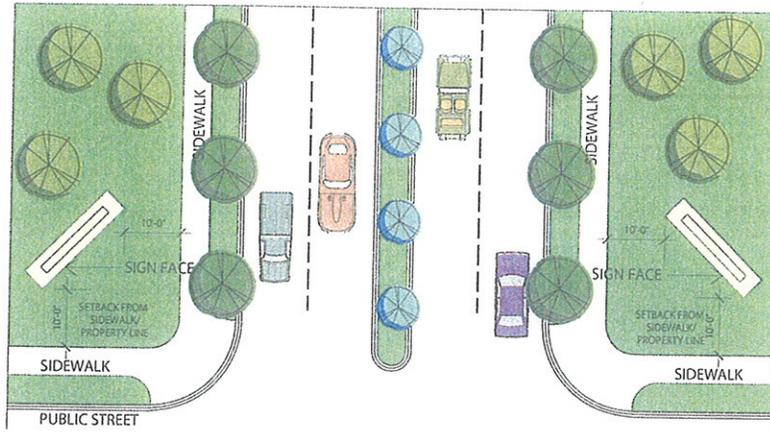
5.13.2 Residential

The following sign standards are applicable to all residential projects within the Central Highway 111 Corridor Specific Plan area:

5.13.2.1 Project Identification Signs

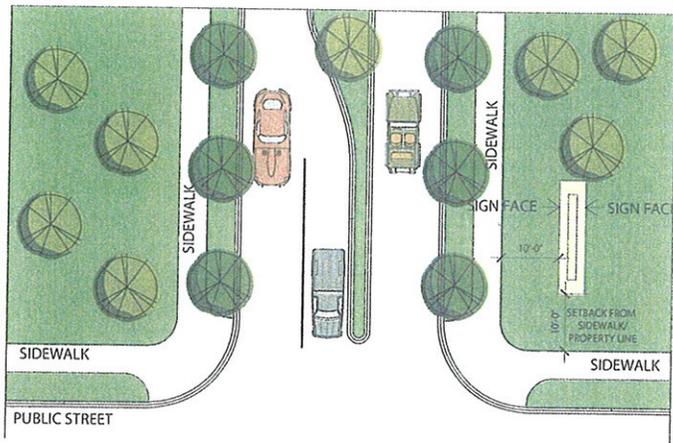
Each residential development shall be permitted one (1) project identification sign per street frontage subject to the following standards:

- a) Sign Type: Monument.
- b) Location: Setback a minimum of 10' from sidewalk and /or property lines.
- c) Maximum Total Sign Face Area: 32 square feet.
- d) Maximum Height: 5 feet.
- e) Illumination: Illumination shall be from an external source, not internally illuminated.



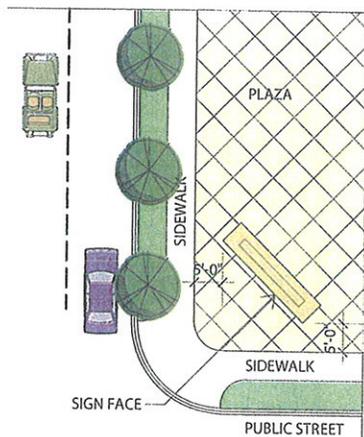
SIGNS MAY BE LOCATED AT BOTH SIDES OF AN ENTRY DRIVEWAY.

MONUMENTATION AT MAJOR ENTRY



SINGLE SIGNS SHALL BE PERPENDICULAR TO THE PUBLIC STREET RIGHT-OF-WAY AND LOCATED EITHER SIDE OF THE DRIVEWAY.

MONUMENTATION AT MINOR ENTRY



MONUMENTATION AT CORNER

6 DESIGN GUIDELINES

6.1 DESIGN GOALS

The design goal of the Central Highway III Corridor Specific Plan is to provide streetscapes, buildings, and landscaping that present an attractive, urban setting for those living, dining, working, and shopping within the project area. A Conceptual Site Plan for the Central Highway III Corridor Specific Plan is illustrated in Exhibit 6-1, showing possible building locations, landscape improvements and roadway configurations. The focus for new development is on architectural interest and high quality materials and finishes. The design guidelines in this section identify components important to the creation of this environment. Following the guidelines are illustrative examples (Exhibit 6-2 through 6-9) of design elements that can be used in the development of properties within the Specific Plan area.

6.2 GUIDELINES FOR DEVELOPMENT

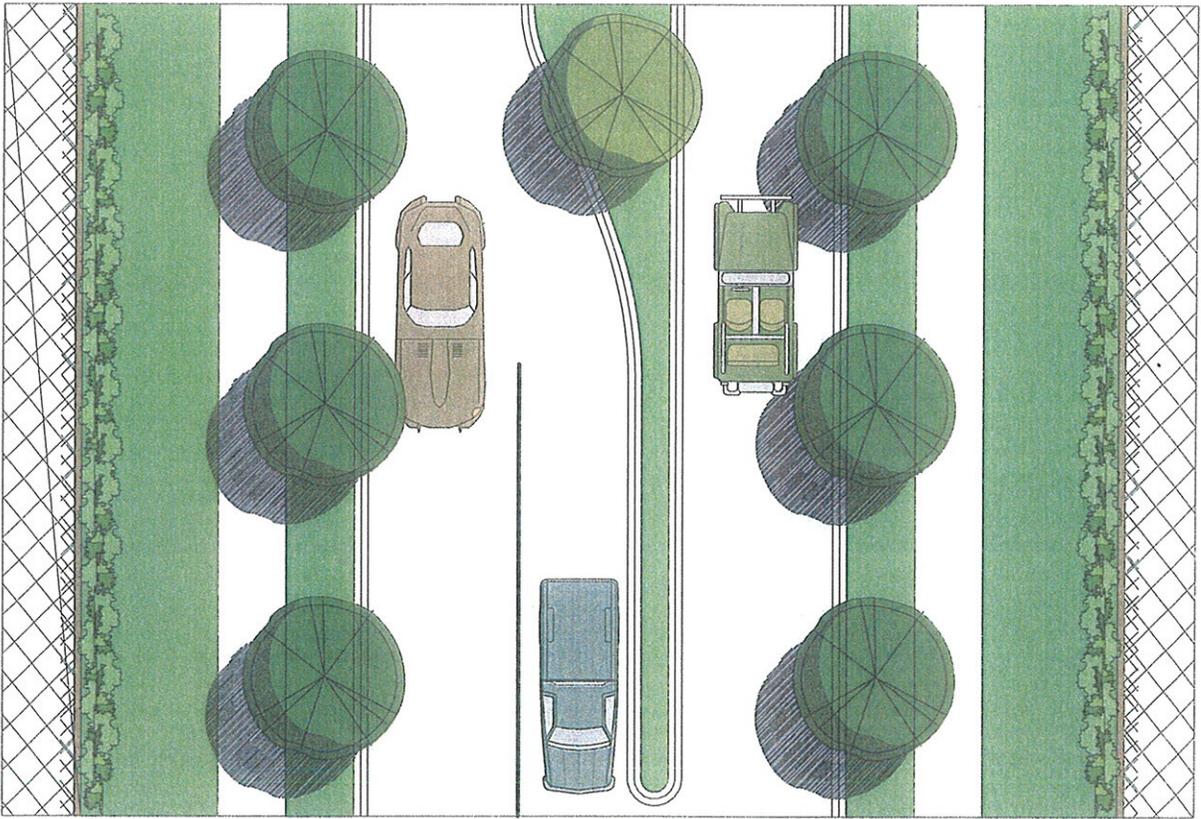
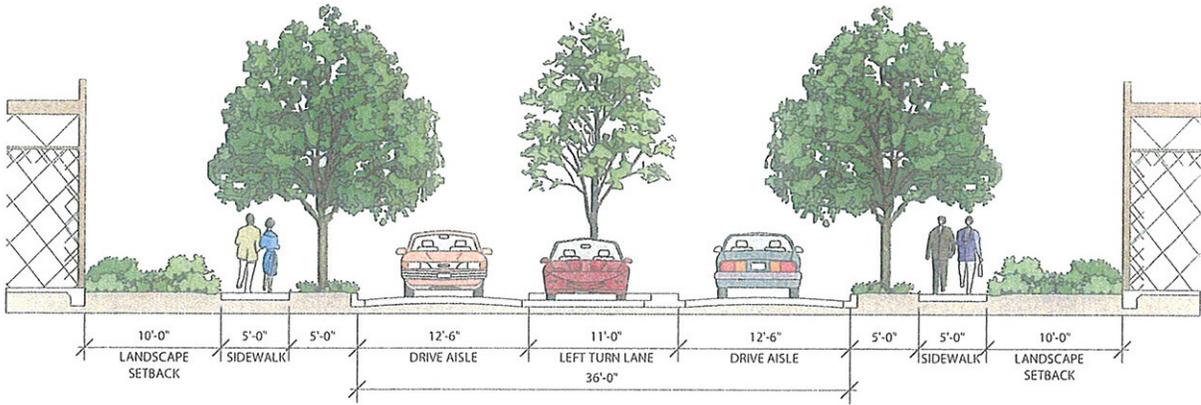
6.2.1 Commercial

Listed below are objectives that should be closely followed for new commercial developments.

Building Form

- The proposed project shall consider the arrangement of buildings, parking, circulation and landscaping on adjacent sites. Buildings and open spaces should be located for the mutual advantage of access, circulation, open space, sunlight and preservation of views.
- Adjacent projects should be connected whenever possible to promote ease of access by pedestrians and automobiles and reduce traffic movements on adjacent streets. In some cases only a sidewalk connection may be necessary while in other cases the provision of shared access and parking will be appropriate.
- Large projects with multiple buildings should consider clustering buildings around areas of useable open space. This provides opportunities to create plazas or pedestrian malls and prevents long rows of buildings. When clustering is impractical, a visual link between separate buildings should be established. This link can be accomplished through the use of an arcade system, trellis or other open structure and textured walkways.

[This page intentionally left blank]



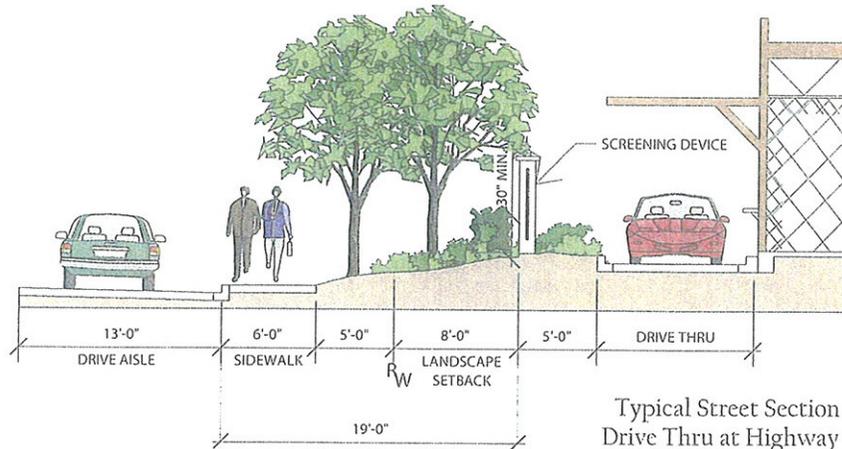
Section-I

Section & Plan at Minor Entry
Central Highway III Corridor Specific Plan

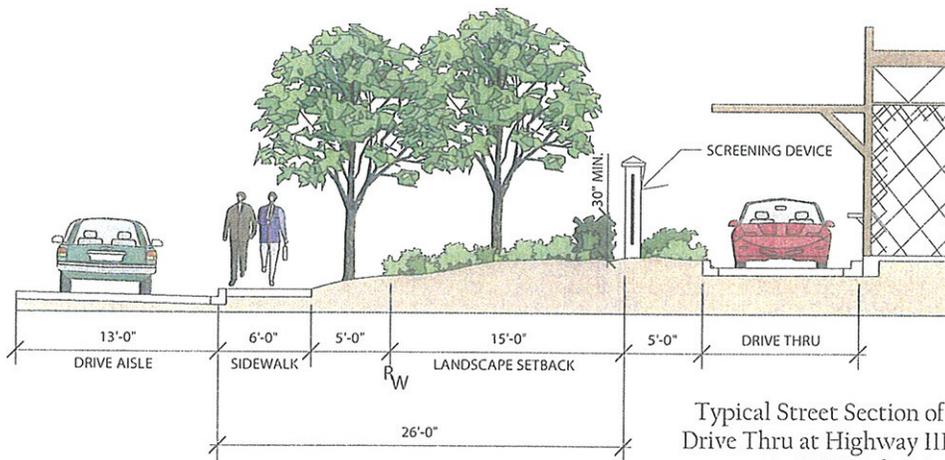
5-3

not to scale

March 2006

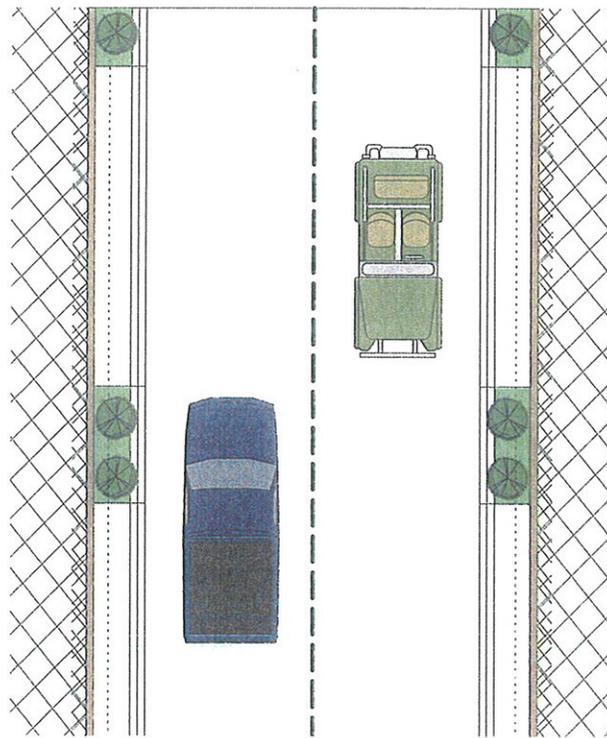
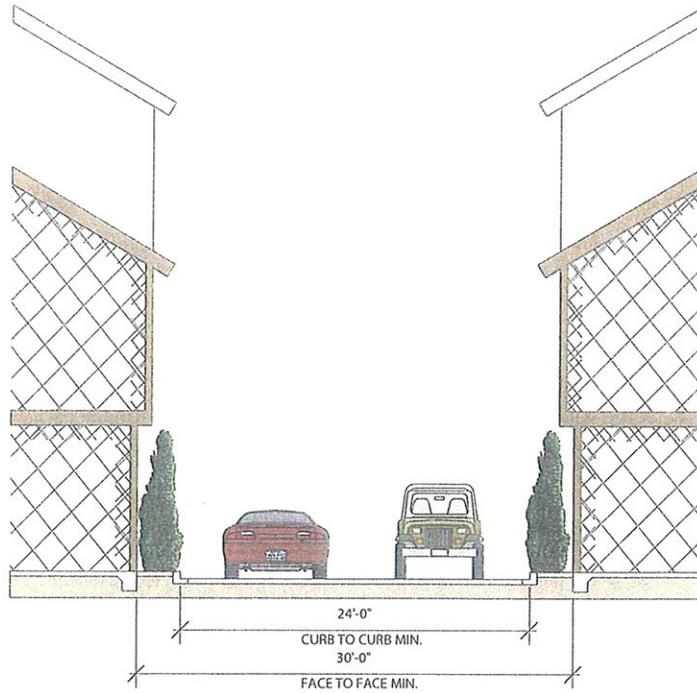


Typical Street Section of Drive Thru at Highway 111 in CC-SP district

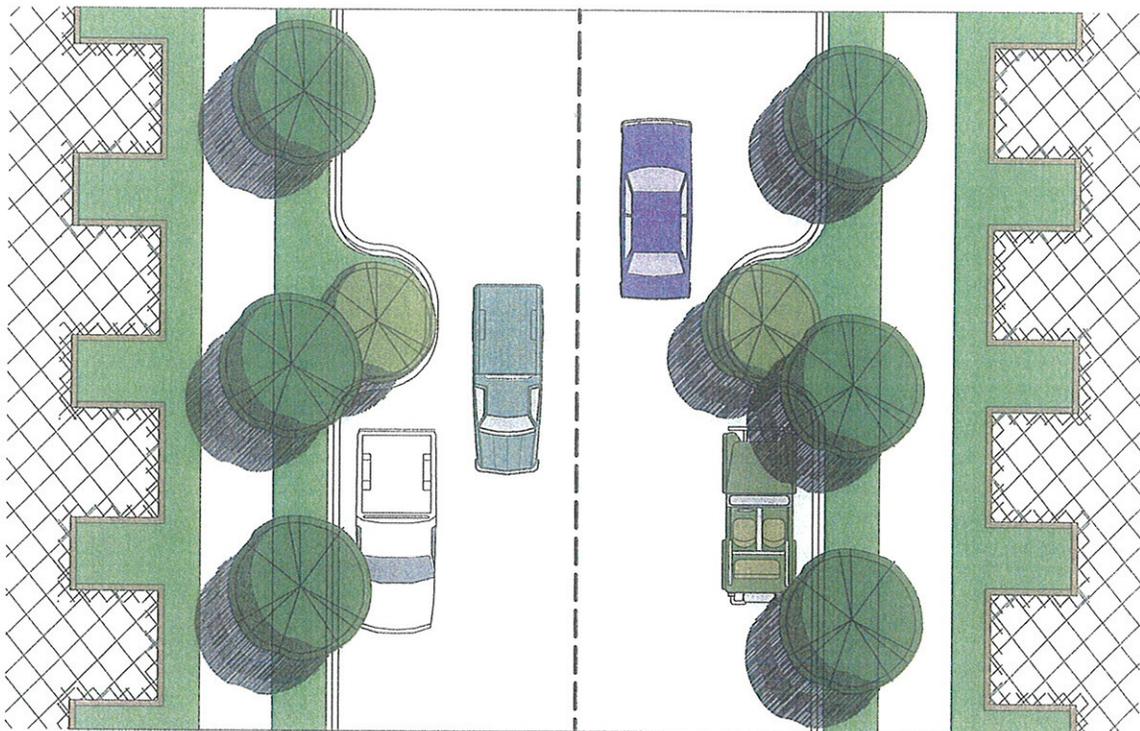
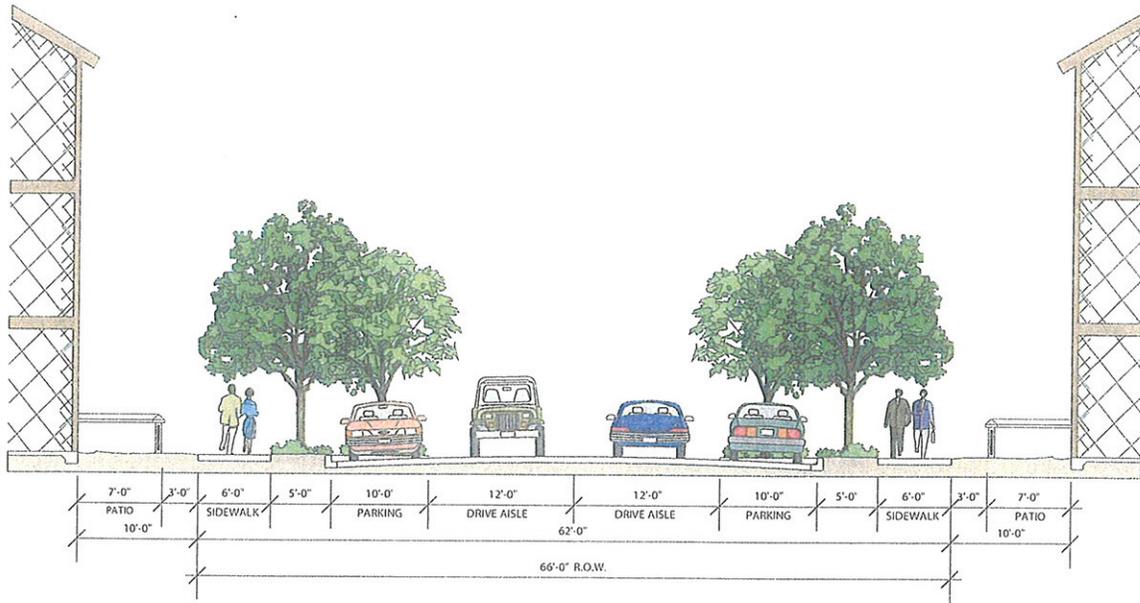


Typical Street Section of Drive Thru at Highway 111 in NC-SP district

Section-J
 Typical Street Section of Drive Thru
 Central Highway 111 Corridor Specific Plan

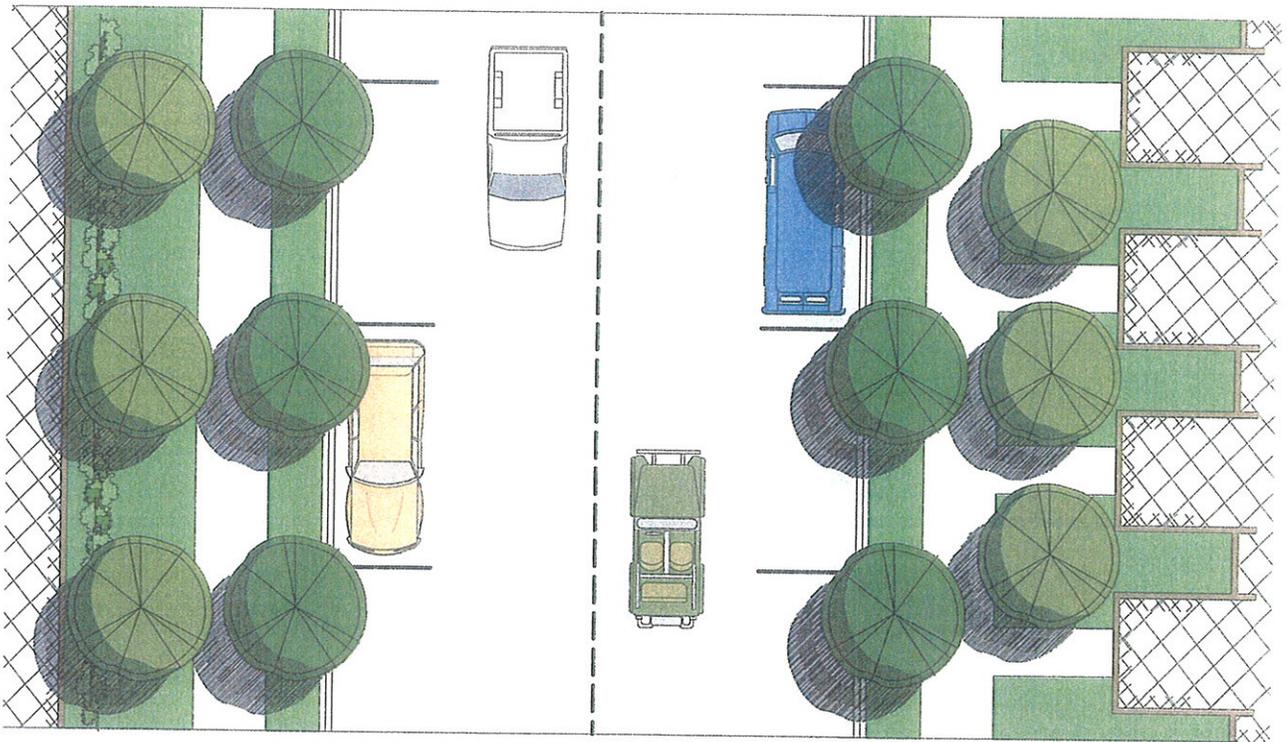
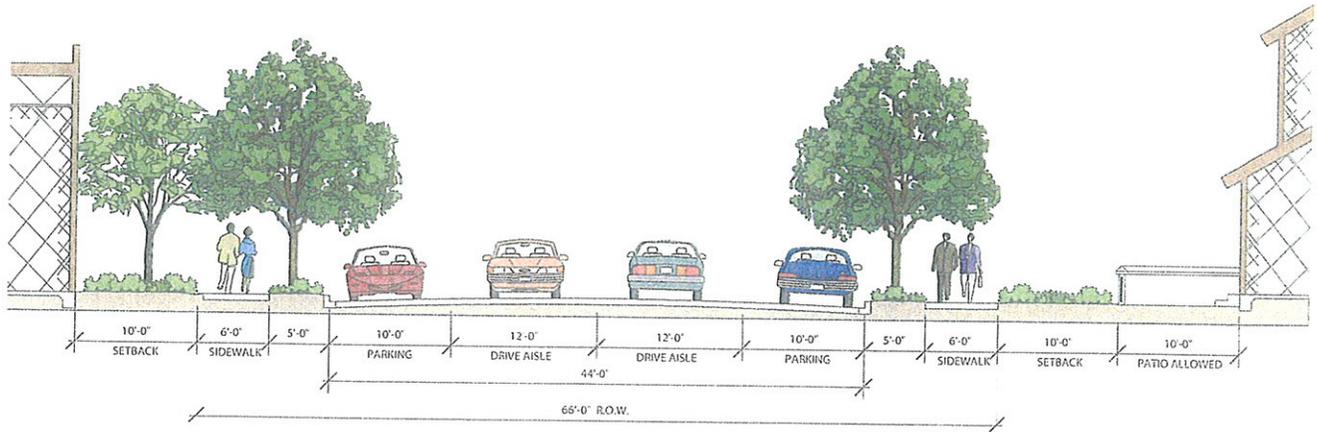


Section-K
Typical Alley Section
Central Highway III Corridor Specific Plan



Section-L
Typical Residential Street Section
Central Highway III Corridor Specific Plan

5-3

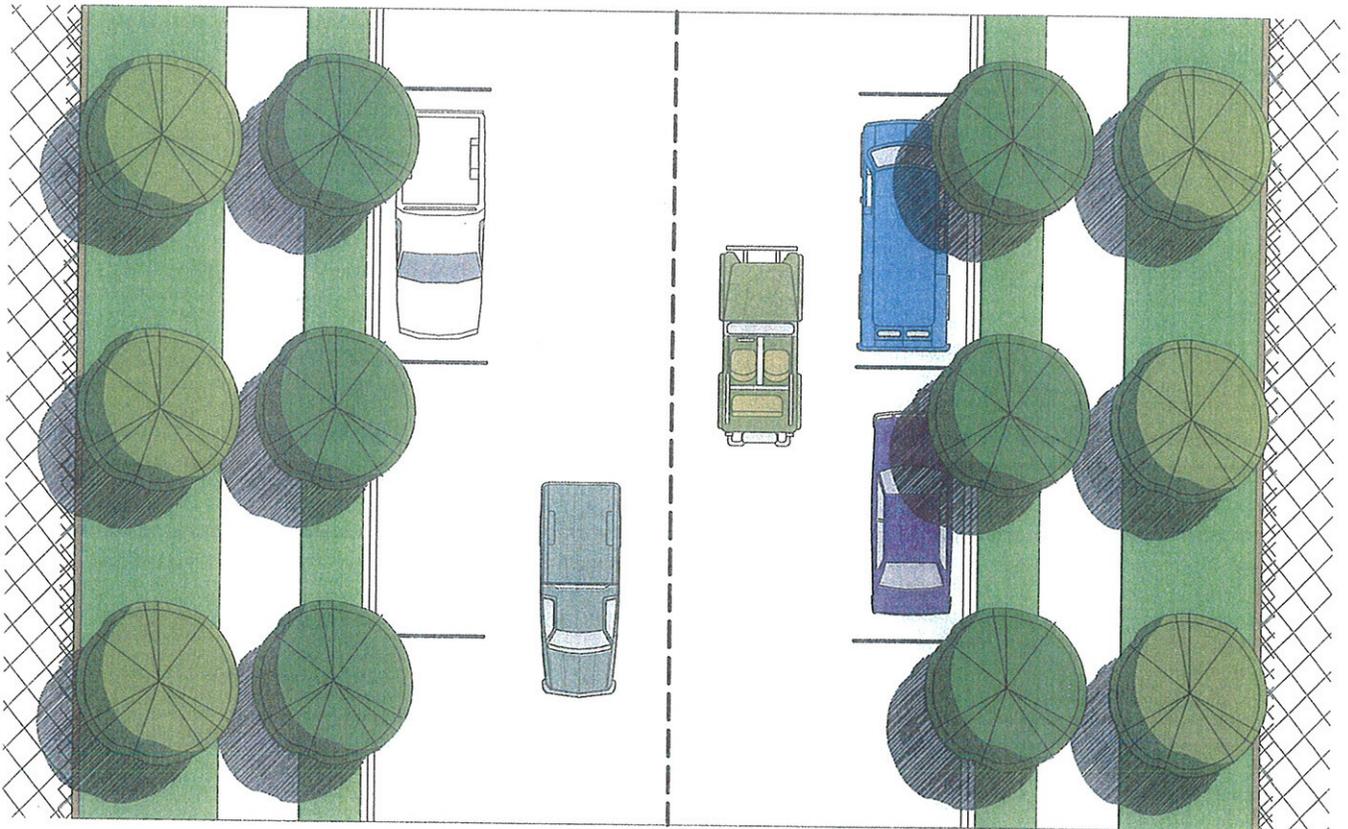
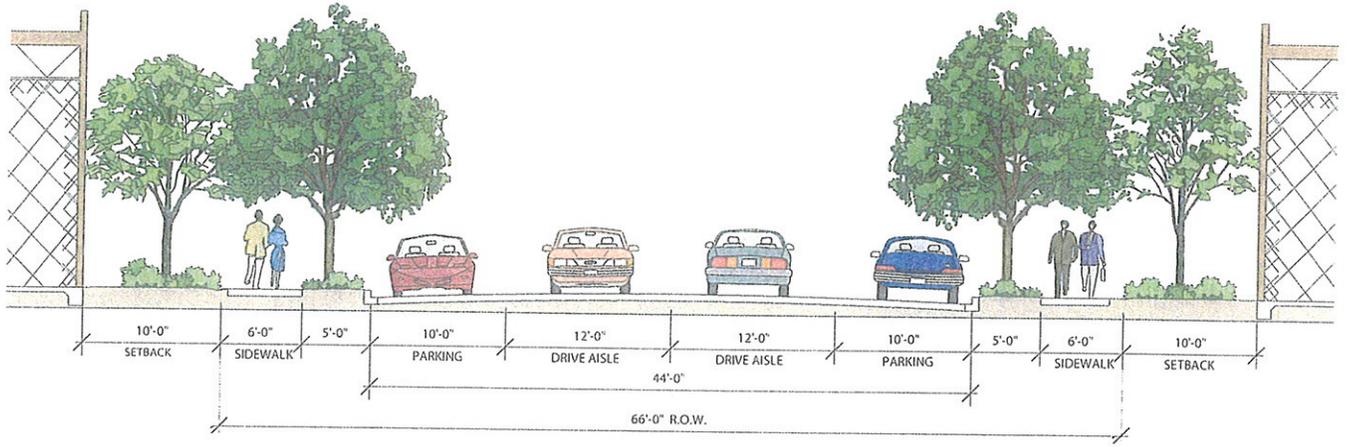


Section-M
Typical Street Section at Las Palmas Road
Central Highway III Corridor Specific Plan

5-3

not to scale

March 2006



Section-N
Typical Commercial Street Section
Central Highway III Corridor Specific Plan

5-3

not to scale

March 2006

- Buildings should be designed to avoid a “boxlike” appearance. Horizontal and vertical wall articulation (recession, reveals, insets, pop-outs) should be expressed through the use of recessed windows and entries, awnings, roof overhangs, second floor setbacks, and covered arcades.
- Vary the planes of exterior walls in depth and/or direction to create visual interest.
- Roof lines with varied levels should be used to enhance the character of commercial projects and minimize the massiveness of large buildings.
- Architectural interest should be created by articulating exterior building walls to create shadow lines and patterns.
- Building entries should be readily visible and special architectural features should emphasize entries.
- Strong architectural elements should be added at the end of long colonnades or storefront areas to create a visual landmark.
- Vertical elements should be included on elevations to define key points or minimize wall mass.
- Vary the planes of the exterior walls in depth and/or direction. Wall planes should not run in one continuous direction for more than 75 feet without an offset.
- It is recommended that the colors used on the exterior of buildings create a contrast between wall color and trim or detail color in order to provide visual interest.
- Entryways should be enhanced by means including the use of recessed doorways, incorporation of eaves, awnings, or canopies, and the use of doorway surround materials that contrast with those of major wall surfaces.
- Tower elements or other vertical architectural features on “ends” of buildings should be incorporated.
- Side and rear elevations should be treated consistently with the building façade for overall architectural continuity.
- Awnings or similar structures shall be incorporated to provide shade for pedestrians.

Access and Parking

- Driveways between sites should be shared to minimize curb cuts.
- Use enhanced paving to announce vehicular entries; the treatment and color should complement the building architecture and overall site design.
- Screen views of parking facilities from the street and adjacent residential uses. A combination of extensive plantings and decorative boundary walls should block views from and buffer adjacent residential properties.

Service Facilities

- Prohibit placement of utility transformer boxes within yard areas adjacent to a street and/or the public right-of-way.
- Screen views of service, loading and storage facilities from streets and adjacent residential uses; appropriate screening strategies include architectural treatment consistent with on-site buildings, or decorative walls with complementary landscape.

Pedestrian Linkages

- Walkways should be located and configured so that pedestrians moving from parking spaces to buildings are separated from drive aisles to the maximum extent feasible.
- Walkways should be readily distinguishable where they cross drive aisles through the use of durable surface materials such as enhanced concrete, pavers, or similar architectural treatments.
- Walkways should be linked to adjoining properties to encourage pedestrian access.
- Place site accessories such as chairs, tables, trash receptacles so that they do not interrupt connecting walkways.

Outdoor Spaces

- Design outdoor spaces for activity and interaction; seating should be provided with deciduous trees that offer shade from summer sun and access to winter sunlight.
- Incorporate enhanced paving within plazas and outdoor spaces, complementary to enhanced paving elsewhere on-site (i.e., pedestrian

pathways); appropriate materials include: colored or stamped concrete; interlocking pavers, seeded concrete; flagstone pavers; slate banding, etc.

Landscape

- Landscape treatment should complement the building architecture and outdoor spaces. See Section 5.11.3 for plant species.
- Use trees to define landscape areas; trees may provide shade, highlight architectural features, and frame desirable views.
- Encourage the use of vines and other suitable plant materials along site walls and fences to enhance their appearance and to discourage graffiti.

Walls and Fences

- Walls and fences between properties should be discouraged to promote pedestrian access.
- Ensure that walls and fences are built of attractive, durable materials that are consistent with the materials and design of the overall development.
- Design boundary walls and fencing consistent with both the commercial and adjacent residential architecture.

Signage

- Monument signs shall be designed to complement building architecture and landscape improvements; colors and materials shall be consistent with on-site structures.
- Tenant identification signs shall be designed to complement the building architecture and should not obstruct architectural features.
- Sign programs are required to ensure that all signs within a project are coordinated to complement one another, but not replicate styles.

6.2.2 Residential

New residential developments should be designed to address the objectives listed below. The architectural design should be compatible with and complement existing developments on the same site or adjacent parcels.

Building Form

- Buildings should be articulated with variations or interruptions of the surface or planes through the use of staggered vertical planes, multiple rooflines, or other similar devices.
- All sides of the building should be articulated with the same level of detail as the front of the building.
- The use of balconies, porches, and patios is encouraged for aesthetic value and for useable private open space.
- Roofs should incorporate varying pitches and heights.
- Exterior stairways should complement the architecture and be integrated into the overall design of the project.

Access and Parking

- On-site parking should be hidden from view from the street.

Service Facilities

- Adequately screen trash enclosures from adjacent uses and open space areas with landscaping or decorative walls.
- Architectural screening elements should be constructed of the same materials and finishes as the primary building.
- Trash enclosures should be located to allow access for refuse collection but should not block vehicular or pedestrian circulation.

Pedestrian Linkages

- All residential development should incorporate pedestrian connections to open space areas, recreation facilities, adjoining residential uses across property lines and commercial uses.
- Pedestrian connections should be clearly visible and enhanced with decorative paving, landscaping, decorative trellis and arbor features.
- Entryways should provide direct access to the street, either individually or for the entire building. Entryways should be architecturally enhanced to compliment the structure.

Outdoor Spaces

- Outdoor seating, tables, and umbrellas, water features, landscaping, gazebos, or other features are encouraged in common open space areas and should be consistent with the architectural style of the project.
- Design outdoor spaces for activity and interaction; trees should offer shade from summer sun, and access to winter sunlight.

Landscaping

- Landscaping should enhance the building design, enhance public views and spaces and provide buffers where needed. Only plant species that are adapted to the climate of the Coachella Valley are to be used.
- Provide coordinated landscape materials that add scale, texture, and color to a development.
- Landscape buffering should be incorporated along the edges between residential development and surrounding arterial and collector streets.
- Tree placement should provide adequate spacing to allow for the normal growth of canopies.

Walls and Fences

- Walls and fences shall be constructed of attractive and durable materials, including, but not limited to, wrought iron and decorative block. Wall colors and materials shall be compatible with the surrounding architecture. Hedges and other landscape elements can also serve as attractive buffers and screens.
- Exterior sides of walls and fences should be screened where appropriate with vine treatments and should be equipped with an automatic irrigation system to help maintain vine growth. The vine treatments should be used to minimize attractive nuisances, particularly graffiti, and to minimize the expansiveness of blank walls.

6.3 SITE LIGHTING

As with signs, landscape, and architecture, improving the quality of exterior lighting can contribute to the perception of quality. All exterior on-site lighting shall be shielded and confined within site boundaries. No direct rays or glare should be permitted to shine onto public streets or adjacent sites.

Lighting should be designed to maintain nighttime safety, utility, security and productivity. Lighting should also minimize glare and light trespass, conserve energy, and curtail the degradation of the night time visual environment.

All lighting fixtures should be appropriate in scale, intensity, and height to the use it is serving.

Basic lighting types include but are not limited to:

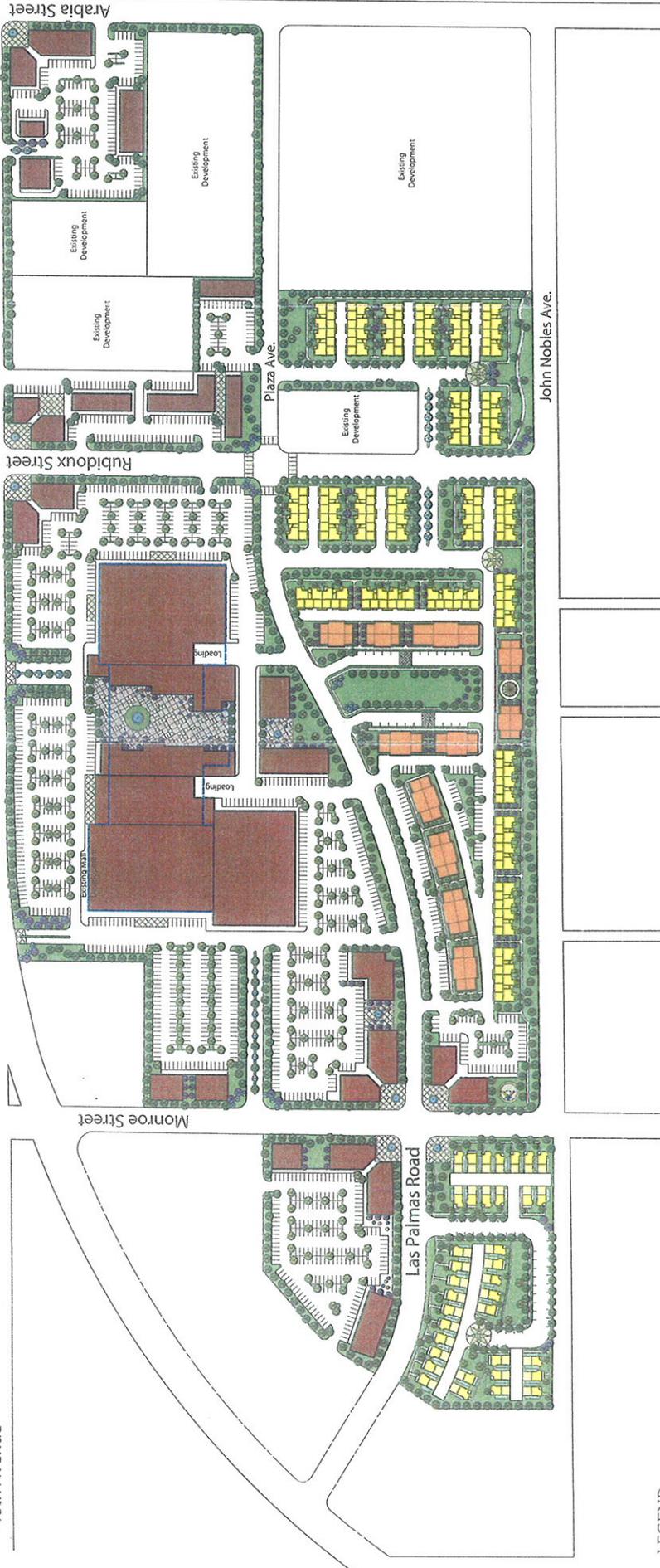
- Street pole lighting along circulation paths;
- Decorative fixtures at entries;
- Pole lighting in parking lots;
- Ground-mounted lights in landscape areas.

6.4 EXTERIOR BUILDING MATERIALS

No single exterior material or color scheme should dominate the architecture of a street or open space in the project. Diversity of architectural styles are to be used as well as a variety of materials such as stucco, stone, tile, enhanced concrete, metal, and glass.

46th Avenue

Highway 111



LEGEND

- Commercial
- Residential
- Mixed Use

not to scale



Conceptual Site Plan
 Central Highway III Corridor Specific Plan

6-1

March 2006



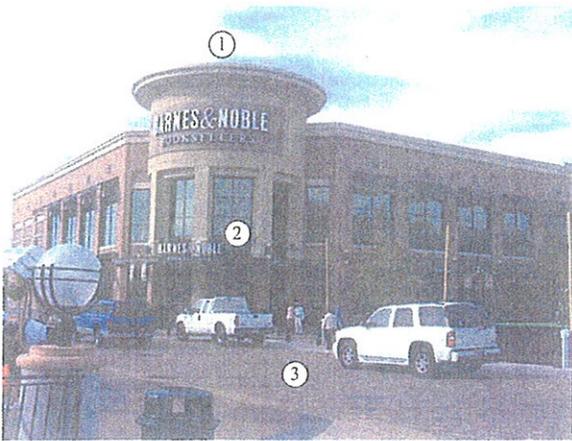
- ① Tenant identification signs shall be designed to complement the building architecture and should not obstruct architectural features.
- ② Building entries should be readily visible and special architectural features should emphasize entries.
- ③ Place site accessories such as chairs, tables, trash receptacles so that they do not interrupt walkways.



- ① Use of trellis structures provide shading, highlights architectural features, and frames desirable views.
- ② Tenant identification signs shall be designed to complement the building architecture and should not obstruct architectural features.
- ③ Enhanced paving announces vehicular entries; treatment and color should complement the building architecture and overall site design.



- ① Use trees to define landscape areas; trees may provide shade, highlight architectural features, and frame desirable views.
- ② Architectural interest should be created by articulating exterior building walls to create shadow lines and patterns.
- ③ Incorporate enhanced paving on pedestrian pathways to create readily distinguishable areas of travel.



- ① Strong architectural elements should be added at the end of long colonnades or storefront areas to create a visual landmark.
- ② Building entries should be readily visible and special architectural features should emphasize entries.
- ③ Use enhanced paving to announce vehicular entries; the treatment and color should complement the building architecture and overall site design.



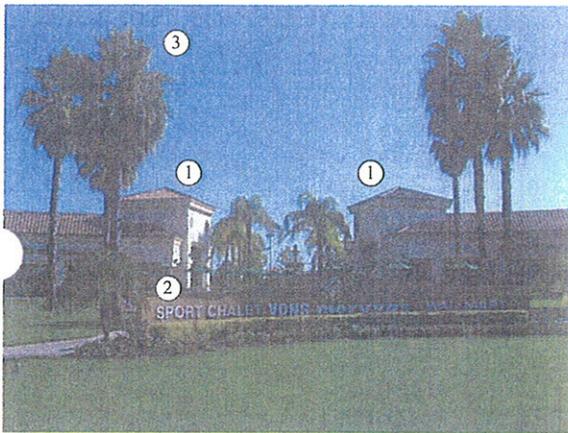
- ① Roof lines with varied levels should be used to enhance the character of commercial projects and minimize the mass of large buildings.
- ② Colors used on the exterior of buildings create a contrast between wall color and trim or detail color in order to provide visual interest.
- ③ Towers or other architectural features on 'ends' of buildings should be incorporated.



- ① Landscape treatment should create a visual barrier between pedestrian areas and parking.
- ② Vary the planes of the exterior walls in depth and/or direction. Wall planes should not run in one continuous direction for more than 75 feet without an offset.
- ③ Design outdoor spaces for activity and interaction; seating should be provided with deciduous trees that offer shade from summer sun and access to winter sunlight.



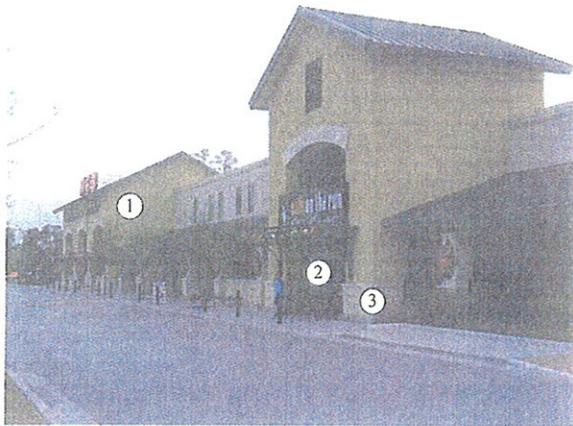
- ① Strong architectural elements should be added at the end of long colonnades or storefront areas to create a visual landmark.
- ② Creation of common plaza spaces for social gathering accommodates various social activities - dining, conversing, resting. Use of fountains, water features, or public art adds visual interest to plaza area.
- ③ Incorporate enhanced paving within plazas and outdoor spaces, complementary to enhanced paving elsewhere on-site.



- ① Building orientation and setback suitable for demarcating important street intersection.
- ② Monument signs shall be designed to complement building architecture and landscape improvements; colors and materials shall be consistent with on-site structures.
- ③ Use trees to define landscape areas; trees may provide shade, highlight architectural features, and frame desirable views.



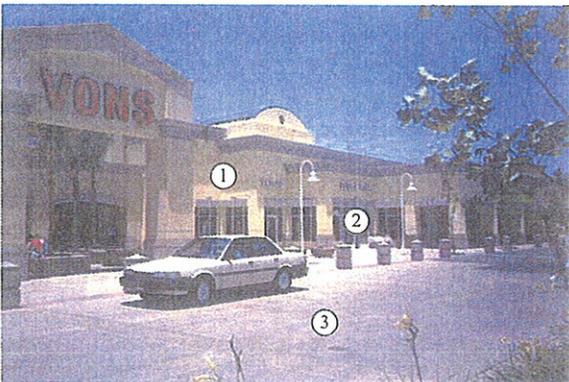
- ① Tower elements or other vertical architectural features on ends of buildings should be incorporated.
- ② Monument signs shall be designed to complement building architecture and landscape improvements; colors and materials shall be consistent with on-site structures.
- ③ Prohibit placement of utility transformer boxes within yard areas adjacent to a street and/or the public right-of-way.



- ① Strong architectural elements should be added at the end of long colonnades or storefront areas to create a visual landmark.
- ② Building entries should be readily visible and special architectural features should emphasize entries.
- ③ Use enhanced building material at the pedestrian level to decrease building scale.



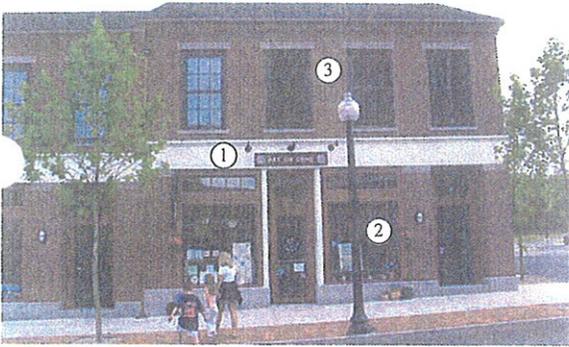
- ① Roof lines with varied levels should be used to enhance the character of commercial buildings and minimize the mass of large buildings.
- ② Colors should be used on the exterior of buildings to create a contrast between wall color and trim color in order to provide visual interest.
- ③ Towers or other architectural features on ends of buildings should be incorporated.



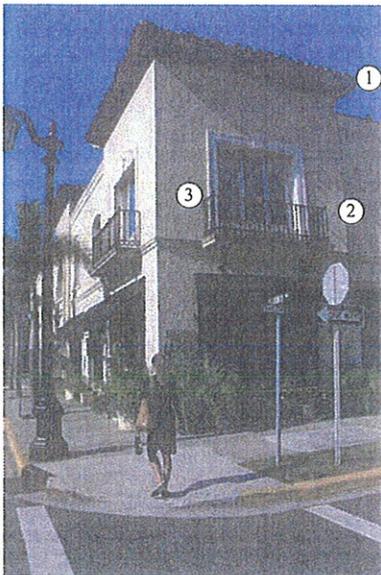
- ① Vary the planes of the exterior walls in depth and/or direction. Wall planes should not run in one continuous direction for more than 75 feet without an offset.
- ② Orient in-line shops and retail space in front of big boxes to decrease building scale.
- ③ Enhanced paving should be used to define building entry.



- ① Provide a sensitive transition to adjacent residential buildings; a step-back in building height and/or variation in building mass may be appropriate.
- ② Design buildings with three-dimensional quality and scale to complement adjacent retail and residential uses.
- ③ Use architectural details to enhance building appearance; careful detailing is especially appropriate at the base of buildings, along cornices, eaves, parapets or ridgetops, and around entries and windows.



- ① Tenant identification signs shall be designed to complement the building architecture and should not obstruct architectural features.
- ② Introduce three-dimensional interest at the street level; as feasible, buildings should provide transparency and openness at the ground level.
- ③ Avoid monotonous or blank facades on all sides of the building; use fenestration (windows and doors) and/or a change in wall plan / modulation, texture, color, etc. to articulate building wall.



- ① Sculpted roof forms and attractively detailed cornices complement the composition of the buildings and the surrounding area.
- ② Vary the planes of exterior walls in depth and/or direction to create visual interest.
- ③ Discourage the use of visibly prefabricated veneers; the following materials are among those recommended: stucco, exterior plaster, wood siding, tile, stone.



- ① Buildings should be articulated with variations or interruptions of the surface or planes through the use of staggered vertical planes, multiple roof lines, or other similar devices.
- ② Landscaping should enhance the building design, enhance public views and spaces and provide buffers where needed.
- ③ Outdoor seating, tables, and umbrellas, water features, landscaping, gazebos, or other features are encouraged in common open space areas and should be consistent with the architectural style of the project.

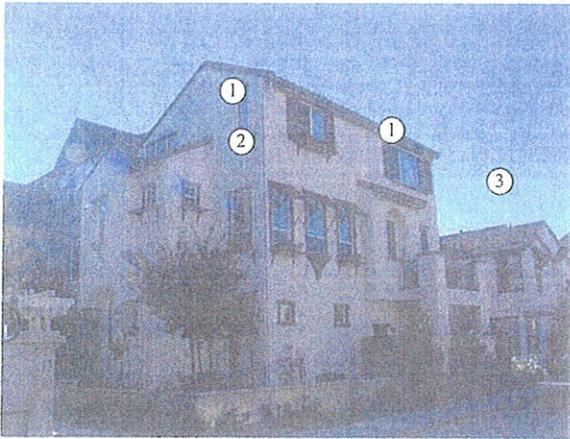


- ① Roofs should incorporate varying pitches and heights.
- ② Building fenestration (windows and doors) should be in proportion to building massing.
- ③ Patio walls should compliment building architecture and define private open space.



- ① Colors should be used on the exterior of buildings to create a contrast between wall color and trim color in order to provide visual interest.
- ② Entryways should provide direct access to the street, either individually or for the entire building. Entryways should be architecturally enhanced to compliment the structure.
- ③ Provide coordinated landscape materials that add scale, texture, and color to a development.

Design Guidelines for
Attached 2-3 Story Residential Buildings
Central Highway III Corridor Specific Plan



- ① Design treatment on the sides of the building should be articulated with the same level of detail as the front of the building.
- ② Buildings should be articulated with variations or interruptions of the surface or planes through the use of staggered vertical planes, multiple rooflines, or other similar devices.
- ③ Roofs should incorporate varying pitches and heights.

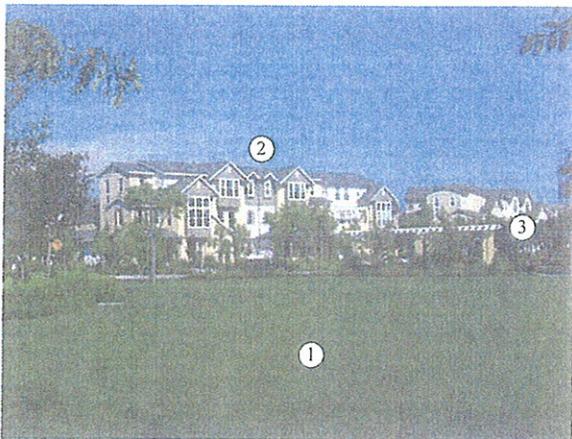


- ① A landscape buffer should be incorporated along the edges between residential development and surrounding streets.
- ② The use of balconies, porches, and patios is encouraged for aesthetic value and for useable private open space.
- ③ Entryways should provide direct access to the street, either individually or for the entire building. Entryways should be architecturally enhanced to compliment the structure.



- ① Employ high quality materials that are durable, long-lasting, and aesthetically pleasing.
- ② Architectural interest should be created with the use of recessed windows to create shadow lines and patterns.
- ③ Landscaping should enhance the building design, public views, and provide buffers where needed.

Design Guidelines for
 Single Family 3-Story Residential Buildings
 Central Highway III Corridor Specific Plan



- ① Use of open space creates an intimately scaled outdoor area or “village green” that can be used for various public events such as civic ceremonies, festivals, live performances as well as daily outdoor seating and eating.
- ② Residential structures should face common open space areas.
- ③ Pedestrian connections should be clearly visible and enhanced with decorative paving, landscaping, decorative trellis and arbor features.



- ① Open space should include pedestrian amenities (i.e.- site furnishings, shading devices, picnic tables, etc..) that are integrated into the overall unified design.
- ② Design outdoor spaces for activity and interaction.
- ③ Landscaping should be used to define the various uses of the village green.



- ① All new pedestrian paths, walkways, sidewalks, etc. shall meet current A.D.A. Standards.
- ② Incorporate ornamental lighting along all sidewalks.
- ③ Design outdoor spaces for activity and interaction; seating should be provided with deciduous trees that offer shade from summer sun and access to winter sunlight.

7 IMPLEMENTATION PLAN

The purpose of this section is to define the implementation measures necessary to carry out the Central Highway III Corridor Specific Plan.

7.1 FINANCING AND FEES

California Government Code Section 65451 sets forth the basic content of specific plans and one of the requirements is to include information relating to project financing and fees.

Various techniques are available for financing the infrastructure and project components. Since certain elements of the infrastructure and project will be for use by the general public, public financing may be appropriate for these elements. All applicable development impact fees and all other related administration fees are collected at the time of issuance of building permits, unless otherwise determined by the City.

7.2 GENERAL PROVISIONS

7.2.1 Applicability

The development standards contained herein provide specific standards for land use development within the Specific Plan area. The Specific Plan supersedes the otherwise applicable City of Indio Zoning regulations unless stated herein to the contrary. Whenever the provisions and development standards contained herein conflict with those contained in the City of Indio Zoning Regulations the provisions of the Specific Plan shall take precedence. Where the Specific Plan is silent, the City of Indio Zoning Regulations shall apply.

7.2.2 Administration

The Central Highway III Corridor Specific Plan serves as the implementation tool for the General Plan as well as the zoning for the project site. The Specific Plan addresses general provisions, permitted uses, development standards and design guidelines.

7.2.3 Interpretation

Development within the Specific Plan area is implemented through the approval of parcel and tentative and/or tract maps. The administrative process described below provides mechanisms for review and approval of development projects within the Specific Plan consistent with the Specific Plan objectives.

Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan is resolved by the Community Development Director or his/her designee in a manner consistent with the goals, policies, purposes and intent established in this Specific Plan.

7.2.4 Implementation

All development proposals within the Specific Plan are subject to the implementation procedures established herein.

7.2.5 Specific Plan Revisions

Revisions to the Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 65453(a) of the Government Code. Revisions are processed pursuant to the provisions of the Government Code for Specific Plan and the City of Indio Zoning Regulations. In the event the proposed revisions require supplemental environmental analysis, pursuant to the California Environmental Quality Act (CEQA), the applicant is responsible for preparing the necessary CEQA documentation.

7.2.6 Minor Modifications

Changes to, or revisions of the land use plan, design guidelines, and development standards, subsequent to adoption of this Specific Plan and/or changes to development plans previously approved pursuant to the Site Plan Review process that generally conform to the goals, policies, purposes, and intents, established herein and are not detrimental to the public health, safety, convenience, or welfare may be approved by the Community Development Director as Minor Modifications consistent with the procedures set forth below.

In no case, shall minor modifications result in an increase in density, increase in height, reduction in setback, decrease in parking ratios, or change of use in a manner that would be inconsistent with the intent of the Specific Plan.

The following constitute Minor Modifications to the Specific Plan. They are subject to review and approval by the Community Development Director. They do not require amendment to the Specific Plan:

- Change in utility and/ public service provider;
- Minor changes to text or maps intended to clarify Specific Plan information, provided such changes do not add density or modify the development policies or standards of the Specific Plan;
- Changes to landscape materials, wall materials, entry design, streetscape design which are consistent with the conceptual design set forth in the design guidelines contained with the Specific Plan;

- Changes to the design guidelines which are intended to be conceptual in nature only, and are intended to be flexible in implementation;
- Minor changes necessary to accommodate proposed tenant improvements provided that the total building area of the commercial components is not increased.
- Other modifications of a similar nature to those listed above as determined by the Community Development Director.

7.2.7 Appeals

All appeals of the Community Development Director's decisions pertaining to this Specific Plan shall be made to the Planning Commission. The applicant or any other entity shall have the right to appeal by filing an application on forms provided by the City of Indio in accordance with the City of Indio Municipal Code.

7.2.8 Maps

Approval of maps may occur with or subsequent to the adoption of the Specific Plan creating parcels for development. The project may include parcel map(s), lot line adjustments and/or other minor subdivision actions.

7.3 GENERAL PLAN CONSISTENCY

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines and programs with the goals and policies set forth in the general plan (See Appendix B).

The Central Highway 111 Corridor Specific Plan has been prepared in conformance with the goals and policies of the City of Indio General Plan.

[This page intentionally left blank]

APPENDICES

Appendix A.	Definition of Terms
Appendix B.	General Plan Consistency

[This page intentionally left blank]

APPENDIX A. DEFINITION OF TERMS

DEFINITIONS - A

Administrative Office: An office for research, analysis, communication, or management related to the principal use on a site.

Arcade: A principal commercial land use consisting of five (5) or more amusement machines located within one building or structure and operated in exchange for financial or other consideration.

DEFINITIONS - B

Barber/Beauty Shops: A business where haircuts, hairdressing, facials, and manicures are done.

Bed and Breakfast Inn: A dwelling unit, which provides lodging and meals for temporary overnight occupants, and in which there are no more than five guest rooms. The use must be managed and operated solely by the owner of the property. Meals are not restricted to breakfast only, but no cooking facilities shall be allowed in the guest rooms. A guest may not stay in the dwelling for more than 14 consecutive days.

Billboard: An off-premise sign that identifies or communicates a commercial or non-commercial message related to an activity conducted, a service rendered, or a commodity sold at a location other than where the sign is located.

Business and Professional Offices: Includes establishments that provide services that include, but are not limited to, property and business administration, engineering services, architectural services, planning services, financial planning services, and accounting services.

DEFINITIONS - C

Caretaker's Residence: An accessory residential use for the purpose of the twenty-four (24) hour maintenance or security of a nonresidential use.

Car wash: A lot or portion of a lot used for the washing, waxing and/or polishing of motor vehicles. Such use may include interior cleaning of vehicles such as vacuuming, but shall not include other uses associated with an automobile service station.

Check Cashing: A business, other than a bank, credit union or other similar financial institution, that cashes, sells and/or processes checks,

money orders, electronic money transfers and other financial documents for a fee.

Convenience Store: An establishment selling a limited range of food, beverage, drug goods, hardware, and/or house goods, that often includes ready-to-consume food and beverage products for consumption off the premises.

DEFINITIONS - D

Day Care Center: A day care facility other than a family day care, including infant centers, preschools, and extended day care facilities. Such a facility must provide care to children in need of personal services, supervision or assistance essential for sustaining the activities of daily living or for the protection of the individual on less than a 24-hour basis.

Department Store: A business selling a wide variety of durable goods and arranged in several departments.

DEFINITIONS - F

Family Day Care: A home which regularly provides care, protection, and supervision of 14 or fewer children, in the provider's own home, for periods of less than 24 hours per day.

Financial Institution: A use providing financial services to individuals, firms, or other entities; includes banks, savings and loan institutions; loan and lending activities; and similar services.

DEFINITIONS - G

Grocery Store: An establishment primarily engaged in selling food for home preparation and consumption, and may include butcher shops with no slaughtering.

DEFINITIONS - H

Health Club: Any business in which a range of indoor fitness activities and classes are offered, including weight training, aerobics, gymnastics, swimming, and racquet sports.

Hotel: Any building or portion thereof with access provided through a common entrance, lobby or hallway to six or more guest rooms, and which rooms are designed, intended to be used or are used, rented or hired out as temporary or overnight accommodations for guests.

DEFINITIONS - N

Night Club: A bar, cocktail lounge, discotheque, restaurant, or similar use which includes dancing and/or entertainment, whether such activity is the principal business use or incidental to a primary use.

Nonconforming Structure/Use: A lawfully established structure or use that does not conform to the regulations of this title or to the regulations of this title for the district in which it is located, either on the date of adoption of this Specific Plan or as the result of subsequent amendments to this Specific Plan.

DEFINITIONS - P

Parking Lots/Structures/Garages: The area for the parking of a motor vehicle plus those additional areas required to provide site ingress and egress to and from said area. The area set aside to meet those provisions must be usable and shall have permanent access for off-street parking.

DEFINITIONS - R

Religious Institution: A facility operated for worship or promotion of religious activities, including churches, synagogues, and mosques, religious Sunday-type schools and monasteries, convents, and other religious retreats.

Research and Development: An establishment which engages in scientific research, and the design, development and testing of computer software, and electrical, electronic, magnetic, optical and mechanical components in advance of product manufacturing.

Residential Accessory Structures: Any structure that is customarily part of a residence and is clearly incidental and secondary and does not change the character of the residential use. Residential accessory uses include the storage of vehicles and other personal property, and structures including swimming pools, workshops, studios, greenhouses, garages, and guesthouses (without cooking or kitchen facilities).

Residential, High Density: Multi-family developments of apartments, condominiums, and single-family attached units up to twenty (20) dwelling units per acre.

Residential, Low Density: Single-family detached units up to five (5) dwelling units per acre.

Restaurant: A commercial establishment where food and beverages are prepared, served, and consumed.

Restaurant, Fast Food: Any commercial establishment serving food or drinks, making provisions encouraging consumption of food or beverage at home or on other premises.

DEFINITIONS - S

Schools, Business and Professional: An educational institution offering specialized trade and commercial courses; includes specialized non-degree granting schools such as: dramatic schools; language schools; driver education schools; seminaries and other establishments exclusively engaged in training for religious ministries; and establishments furnishing educational courses by mail.

Sign: Any device, structure, fixture or placard using writing, pictorial presentation, emblem, or flag for the primary purpose of identifying, providing directions or advertising any establishment, product, good or service and is considered: attached to, painted on, or in any other manner represented on a building or other structure or device, and is visible from outside the building or structure, including any and all supporting structures.

Social Club: A nonprofit association of persons who are bona fide members, paying regular dues, and are organized for some common purpose. Facilities typically include meeting halls for club/organization members.

DEFINITIONS - V

Vehicle Repair: A commercial service establishment engaged in repair, alteration, restoration, towing, painting, cleaning or finishing of automobiles, trucks, recreational vehicles, boats and other vehicles.

Veterinary Clinic: A medical facility licensed by the State Department of Public Health for the treatment of household pets. The use may include the overnight care of patients.

APPENDIX B. GENERAL PLAN CONSISTENCY

This Central Highway III Corridor Specific Plan has been developed based on guiding policies outlined in the Indio General Plan. The proposed General Plan Land Use Designations in the project area include Residential High-Density, Neighborhood Commercial, Community Commercial, and Mixed Use- Specific Plan.

Residential High-Density is defined as follows:

Policy LU-1.8

"The Residential High- Density designation allows multi-family developments of apartments, condominiums, and single-family attached units."

The Residential High-Density designation allows a maximum of 20 dwelling units per acre. Planning Areas 4, 5 and 6 are designated as Residential High-Density and comply with Policy LU- 1.8.

Neighborhood Commercial is defined as follows:

Policy LU-3.1

"The Neighborhood Commercial designation provides for convenient small scale shopping and personal service uses in close proximity to residential neighborhoods."

Planning Area 3 is designated as Neighborhood Commercial and the proposed uses and development standards are consistent with Policy LU- 3.1.

Policy LU-3.2

"The Community Commercial designation is intended to provide for general merchandising and retailing establishments that serve the needs of the residents of Indio. The range of services and merchandise will be greater than those found in the neighborhood centers, and the service area will be City-wide compared to neighborhood oriented."

Planning Areas 1 and 7 are designated as Community Commercial and the proposed uses and development standards are consistent with Policy LU- 3.2.

Mixed Use, Specific Plan is defined as follows:

Policy LU-10.3

"The Mixed Use- Specific Plan Designation is intended to allow for a mix of residential and commercial land uses. This designation will be used to allow persons to live close to employment opportunities, and to provide for a

transition from higher intensity commercial uses to more traditional residential developments.”

Planning Areas 2, 8 and 9 are designated as Mixed Use- Specific Plan and the proposed uses and development standards are consistent with Policy LU-10.3.

The Central Highway III Corridor Specific Plan is consistent with the policies described above in that it provides a land use concept, design guidelines and development regulations for a project that includes residential neighborhoods that will provide opportunities to fit the current and future housing needs of Indio, and diverse commercial areas to serve those living in, working in, and visiting Indio.